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# Tadcaster Design Guide & Codes

For Tadcaster Neighbourhood Plan

April 2022

Delivering a better world

## Quality information

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## Revision History

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5	19.04.22	For issue to Locality	Nick Beedie	Associate Director	David Gluck	Neighbourhood Plan Steering Group Chair

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Introduction

01

# 1. Introduction

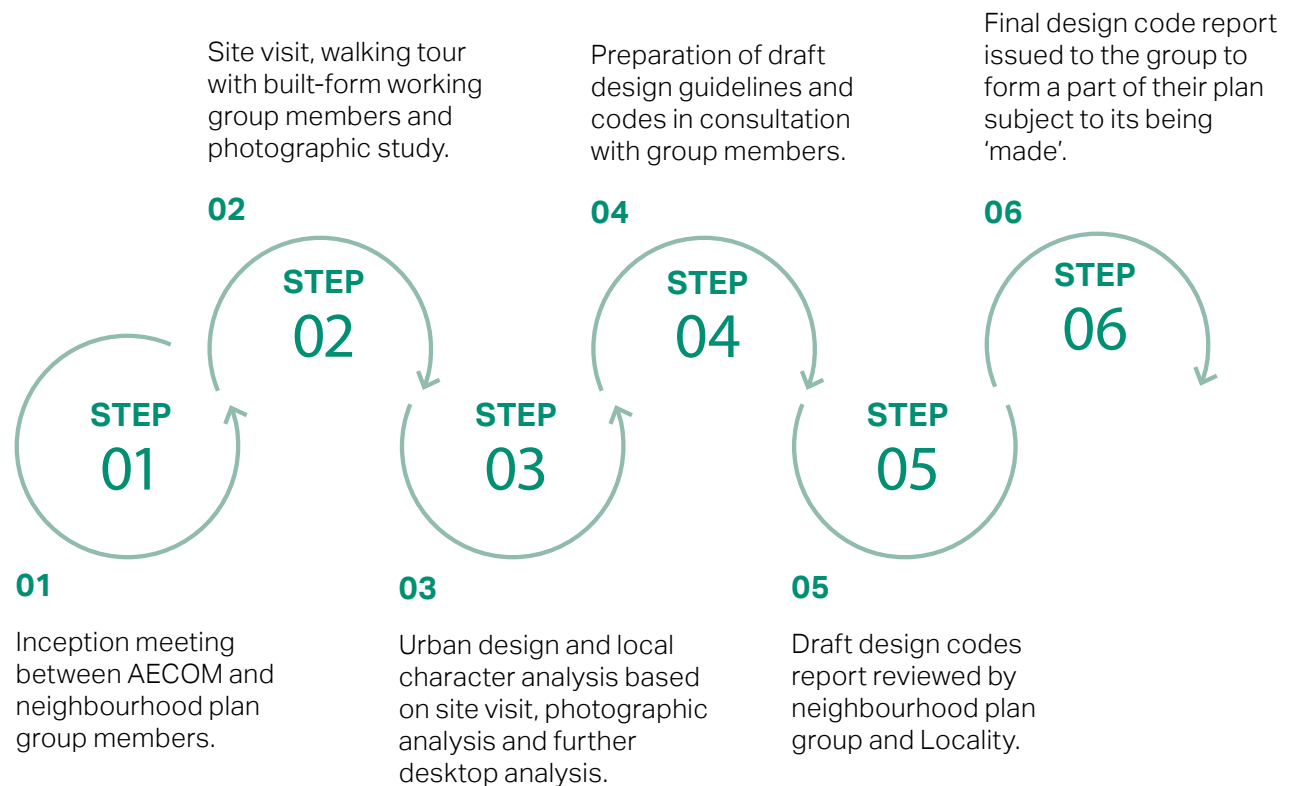
The Neighbourhood Plan group requested support to establish a design guide with a number of design codes to influence the character and design of new development within the neighbourhood area. The area has a housing requirement of around 500 homes which is expected to be met through Local Plan allocations. The design guide covers the whole neighbourhood area with design codes that are site-specific.

The design guidance and design codes are underpinned by an assessment of the urban character across the town (see Section 3). The neighbourhood area analysis (Section 2) addresses constraints in relation to the nearby SSSI (outside the neighbourhood area), flooding and landscape character.

The design guide and codes will help to unlock the development potential of the town by setting out guidance and codes which meet the aspirations of all local stakeholders and supports the delivery of high-quality, sustainable development on site allocations within the development limit which might otherwise be highly contested.

## 1.1 Aims

- To positively influence the character and design of new development within the neighbourhood plan area
- To understand the impact of the SSSI as a constraint to development within the NP Area
- To understand the character of the Conservation Area within the town of Tadcaster
- To understand the character of other areas in and around the town of Tadcaster



## 1.2 Objectives

To achieve the overall aims of the study, the following objectives have been undertaken;

- Assessment of the wider landscape context of the NP Area;
- Assessment of development constraints in relation to the SSSI;
- Review of constraints in relation to the Conservation Area (and emerging draft appraisal, see 1.5.3);
- Review of landscape character study for the whole NP area (see 1.5.3);
- Characterisation of the main settlement of Tadcaster;
- Design guidance covering the whole NP area, focussing on the main settlement; and
- Design codes covering important sites within the Tadcaster.

## 1.3 Area of study

The NP group requested that the main settlement of Tadcaster is the primary focus of the report. However, it is important to understand the settlement context within the surrounding NP Area, which corresponds to the Parish boundary area in this case.

As such we have reviewed existing character studies that cover the whole parish so that this is understood, both in its influence on the design of the town from a landscape character perspective and the statutory designations (Green Belt and SSSI) that affect the town. Both town and country connect and relate in their complementary roles and characteristics.



**Figure 01:** View of John Smith's (now Heineken) brewery



**Figure 02:** View of Tadcaster Bridge over the River Wharfe

- Neighbourhood Plan boundary
- Development limit
- Shopping and commercial centre
- Key roads



**F.3** | **Figure 03:** Map showing the neighbourhood plan area of Tadcaster

## 1.4 Who should use the guide

The Design Code should be a valuable tool in securing context driven, high-quality development in Tadcaster. It will be used in different ways by different actors in the planning and development process, as summarised in the table.

A valuable way they can be used is as part of a process of co-design and involvement that further understands and takes account of local preferences and expectations of design quality. In this way the guidance and codes can help to facilitate conversations on the various topics that should help to align expectations and help understand the balancing of key issues. A design code alone will not automatically secure optimum design outcomes but should help to prevent many of the worst and raise standards.

Potential users	How they will use the design guidelines
<b>Applicants, developers, &amp; landowners</b>	As a guide to community and Local Planning Authority expectations on design, allowing a degree of certainty – they will be expected to follow the Guidelines as planning consent is sought.
<b>Local Planning Authority</b>	As a reference point, embedded in policy, against which to assess planning applications.  The Design Guidelines should be discussed with applicants during any pre-application discussions.
<b>Parish Council or Neighbourhood Plan Group</b>	As a guide when commenting on planning applications, ensuring that the Design Guidelines are complied with.
<b>Community groups &amp; Local Residents</b>	As a reference point embedded in policy, against which to assess planning applications in making comments.
<b>Statutory consultees</b>	As a reference point when commenting on planning applications.

**Table 01:** How will different groups use the guidance



## 1.5 Planning policy and design guidance

There are several national and local planning policy and guidance documents that have been referred to in the development of this design guide and the codes featured in it. This section highlights recent government initiatives such as the National Design Guide and Homes England adoption of Building For a Healthy Life (formerly building for Life 12).

### 1.5.1 National Planning Policy & Guidance (revised July 2021)

The National Planning Policy Framework (NPPF) outlines the Government's overarching economic, environmental and social planning policies for England. The policies within NPPF apply to the preparation of local and neighbourhood plans, and act as a framework against which decisions are made on planning applications.

The NPPF states that a key objective of the planning system is to contribute to the achievement of sustainable development, which will be achieved with reference to three overarching objectives. The parts of the NPPF which are of particular relevance to this Design Code are:

- Part 7 (Ensuring the vitality of town centres)
- Part 12 (Achieving well-designed places)
- Part 13 (Protecting Green Belt land)

Part 12: Achieving well-designed places stresses the creation of high-quality buildings and places as being fundamental to what the planning and development process should achieve. It sets out a number of principles that planning policies and decisions should consider ensuring that new developments are well-designed and focus on quality.

### 1.5.2 National Design Guide (2019) & National Model Design Code (2021)

These documents set out characteristics of well-designed places. They support the ambitions of the NPPF to utilise the planning and development process in the creation of high-quality places and are expected to be used by local authorities, applicants and local communities to establish the design expectations of the Government.

These and other recent national guides which should influence the design and development of places is outlined in the following summary boxes (over page).



### 2019 - National Design Guide DLUHC

The National Design Guide (Ministry of Housing, Communities and Local Government, 2019) illustrates how well-designed places that are beautiful, enduring and successful can be achieved in practice.

It sets out 10 key fundamentals which underpin good design. These are; Context, Identity, Built Form, Movement, Nature, Public Spaces, Uses, Homes & Buildings, Resources and Lifespan. These topics are a useful structure to address many complex and overlapping issues that occur at a range of scales within the built and natural environments.



### 2021 - National Model Design Code DLUHC

This report provides detailed guidance on the production of design codes, guides and policies to promote successful design.

It is set out in sections according to the 10 characteristics of good design from the earlier National Design Guide. This guide should be used as reference for creating new design codes for development, however it is not required that all design codes follow this structure.

The aim of this report is not to repeat this level of work but to add further depth on local places and issues, also in a structured way.



### 2020 - Building for a Healthy Life Homes England

Building for a Healthy Life (BHL) is the new (2020) name for Building for Life, the government-endorsed industry standard for well-designed homes and neighbourhoods. The new name reflects the crucial role that the built environment has in promoting wellbeing. The BHL toolkit sets out principles to help guide discussions on planning applications and to help local planning authorities to assess the quality of proposed (and completed) developments, but can also provide useful prompts and questions for planning applicants to consider during the different stages of the design process.

### 1.5.3 Local Planning Policy & Guidance

Selby District Council and North Yorkshire County Council are the local authorities (providing upper tier services such as highways and transport and lower tier services such as planning respectively). They are expected to merge into a unitary authority in 2023.

The following documents are current references to local policy and guidance that have informed the design guidelines and codes. At the time of writing the emerging local plan is timetabled towards adoption in early 2023.

In the case of the Tadcaster Conservation Area Appraisal this document was being produced in parallel to this study and every effort has been made to accommodate the emerging conclusions of the report within this work. Guidance in this document related to the conservation area will need to be reviewed in the context of the final report.

Local Policy & Guidance	Date
Selby District Council Local Plan	Emerging -timetable indicates adoption at beginning of 2024
Tadcaster Conservation Area Appraisal	February 2021 (Draft)
The Selby District Charter Vision: <i>'A historic brewing town with an unspoilt character that is a good place to live and visit.'</i>	Unknown
Selby District Renaissance Strategic Development Framework (including Tadcaster Masterplan)	Unknown
Selby Landscape Character Assessment prepared by LUC on behalf of Selby District Council	November 2019

**Table 02:** Relevant local policy and guidance

## 1.6 Site visits and engagement

A town walkover was conducted on the 29/4/21 with Mayor of Tadcaster Stephen Cobb and former local architect with Samuel Smith's Brewery Alex Tobin. Both provided valuable perspectives on the current and historic issues, constraints and opportunities, focused on the areas and sites in and around the town centre (including Samuel Smiths Brewery site) and the River Wharfe corridor.

AECOM followed up this informative tour with a wider walking expedition across the town and some of its fringe areas accessed via public rights of way. This broadly circular route around the town provided a visual and photographic study across parts of all of the character areas subsequently defined in this study. Some of the locations that were inspected along the way included; Inholmes Lane; Kelcbar Close; Viaduct Walk; Grange Avenue, Ouston Lane, Rosemary Row, the High Street, Station Rd and Stutton Rd.

A short follow up site visit was undertaken on Tuesday 28th September 2021 to check some details of the character areas and obtain additional photographs.

A draft design guide and codes document was presented to the neighbourhood group on 14th February 2022. A broad discussion was had regarding the scope of the content which was well received and also highlighted some smaller details to update prior to this final version.



**F.4**

**Figure 04:** The Barn, a multi-functional community facility and meeting place



**Neighbourhood Context**

**02**

## 2. Neighbourhood Context

This chapter outlines the broad landscape character, planning constraints and context of the Neighbourhood Plan Area.

### 2.1 Landscape character

Landscape character is defined as the distinct, recognisable and consistent pattern of elements in the landscape. It is these patterns that give each locality its 'sense of place', making one landscape different from another, rather than better or worse. In defining the combinations of components which make each landscape unique, landscape character is a way of thinking about landscape more holistically and objectively, rather than focusing on scenic beauty and subjective responses.

Landscape character can describe the geology, topography, land cover and vegetation, features, degree of openness and views, all of which relate to and inform the origins, setting and make-up of the town. Tadcaster, therefore, is intrinsically connected to its rural hinterland.

There are three landscape character areas surrounding Tadcaster (Selby Landscape Character Assessment, 2019):

- a. West of NP Area: Magnesian Limestone Ridge (greenbelt)
- b. East of NP Area: York Fringe West (Vale Farmland)
- c. South: Wharfe Valley (floodplain)

The relevance of these is to understand the influence on the identity of the town and what is rooted in the place, e.g. local materials, and particularly how it can help inform future development in and around town so that it sits within and relates to its landscape context.

### Key takeaways

**Tadcaster is urbanised but relates closely to its underlying landscape character on the banks of the River Wharfe and in the DNA of its eastern and western extents.**

**The Magnesian limestone ridge to the west provides historic local building material and rising slopes.**

**The Wharfe valley has a strong rural and natural character, bringing nature into the town and danger of flooding.**

**The landscape to the east is flatter, more open and intensively farmed with a strong rural character.**

**Development that can show how it draws influence from these unique characters will add to the sense of place.**



**F.5** | **Figure 05:** Map showing Landscape Character Areas overlaid on the Neighbourhood Plan Area

## 2.2 Designations

The following designations are of great importance when considering development constraints within the Neighbourhood Plan area.

### 2.2.1 Tadcaster Mere SSSI

This statutory designation covers an area of 8.7Ha and is located 500m east of Tadcaster (see map over page).

*Tadcaster Mere encompasses the central part of a former lake basin which extended over an area of about 3km<sup>2</sup>. The sub-surface sediments provide a long record of Devensian Late-glacial and early Flandrian vegetation history. It is particularly important for Late-glacial pollen studies and the opportunity it provides for dating the Escrick moraine upon which it lies.*

(designatedsites.naturalengland.org.uk).

SSSI Impact Risk Zones are used to assess planning applications for likely impacts on SSSIs. If the nature and scale of the proposed development matches one or

more of the development descriptions listed in the right hand margin of the results table alongside a development category then the proposed development has the potential to impact upon a SSSI (see SSSI IRZ User Guidance MAGIC.pdf, defra.gov.uk).

The Local Planning Authority should consult Natural England for advice on how impacts might be avoided or mitigated. Consultations should be sent to consultations@naturalengland.org.uk. If you are a developer, consultant or member of the public preparing to submit a planning application, Natural England can be consulted for pre-application advice on how impacts might be avoided or mitigated. See the gov.uk website for further information on our pre-application discretionary advice service.

### 2.2.2 Tadcaster motte and bailey castle (Scheduled Monument)

This scheduled monument is located to the north of the town centre, within the conservation area. The monument includes a motte and bailey castle situated on the






west bank of the River Wharfe. The area is of high archaeological sensitivity due to the remains of a C11/C12 earthen castle. It may be the core of the Roman settlement (Calcaria) and partial Saxon Burgh. It is currently a fully accessible recreation area.

### 2.2.3 Greenbelt

This non-statutory designation covers the land to the west of the main settlement only, within the NP area and further west. Green belt is a policy for controlling urban growth, preventing sprawl and maintaining openness of the landscape. It covers countryside around settlements such as Tadcaster to indicate where urbanisation will be resisted for the foreseeable future. It aims to maintain an area where agriculture, forestry and outdoor leisure is expected to prevail against development pressure.

Greenbelt designation covers over 50% of the neighbourhood area. It covers the neighbourhood area to the west of the river and town, outside of the town boundary (keyed as the development limit on the following plan on the next page).



-  Neighbourhood Plan area boundary
-  Key Routes
-  Development Limit (of the town)
-  Conservation Area
-  Site of Special Scientific Interest (SSSI)
-  Green Belt



**F.6** Figure 06: Map showing designations in Tadcaster

500m



## 2.2.4 Development Limits

The Selby District Council development limit policy boundary defines the extent of the town. It distinguishes between the urban area and its constituent land uses (such as residential, employment, retail, or community facilities and services) and the countryside and its rural land uses (agriculture, grazing land, outdoor recreational uses, or woodland).

The urban landscape of Tadcaster covers approximately 25% of the Neighbourhood Plan area, on both sides of the River Wharfe.

## 2.2.5 Conservation Area

Conservation areas are areas of special architectural or historic interest that have character or appearance which is desirable to preserve or enhance. Any new development should be designed with consideration for how it will preserve the historic character of the conservation area. Tadcaster conservation area includes features which make a positive or negative

contribution to the special character of the area (see recent consultation draft Tadcaster Conservation Area Appraisal, Feb. 2021).

### Key takeaways

**Tadcaster Conservation Area covers land to the east and west of the River Wharfe, including a stretch of the river and its banks alongside most of the town centre and commercial areas.**

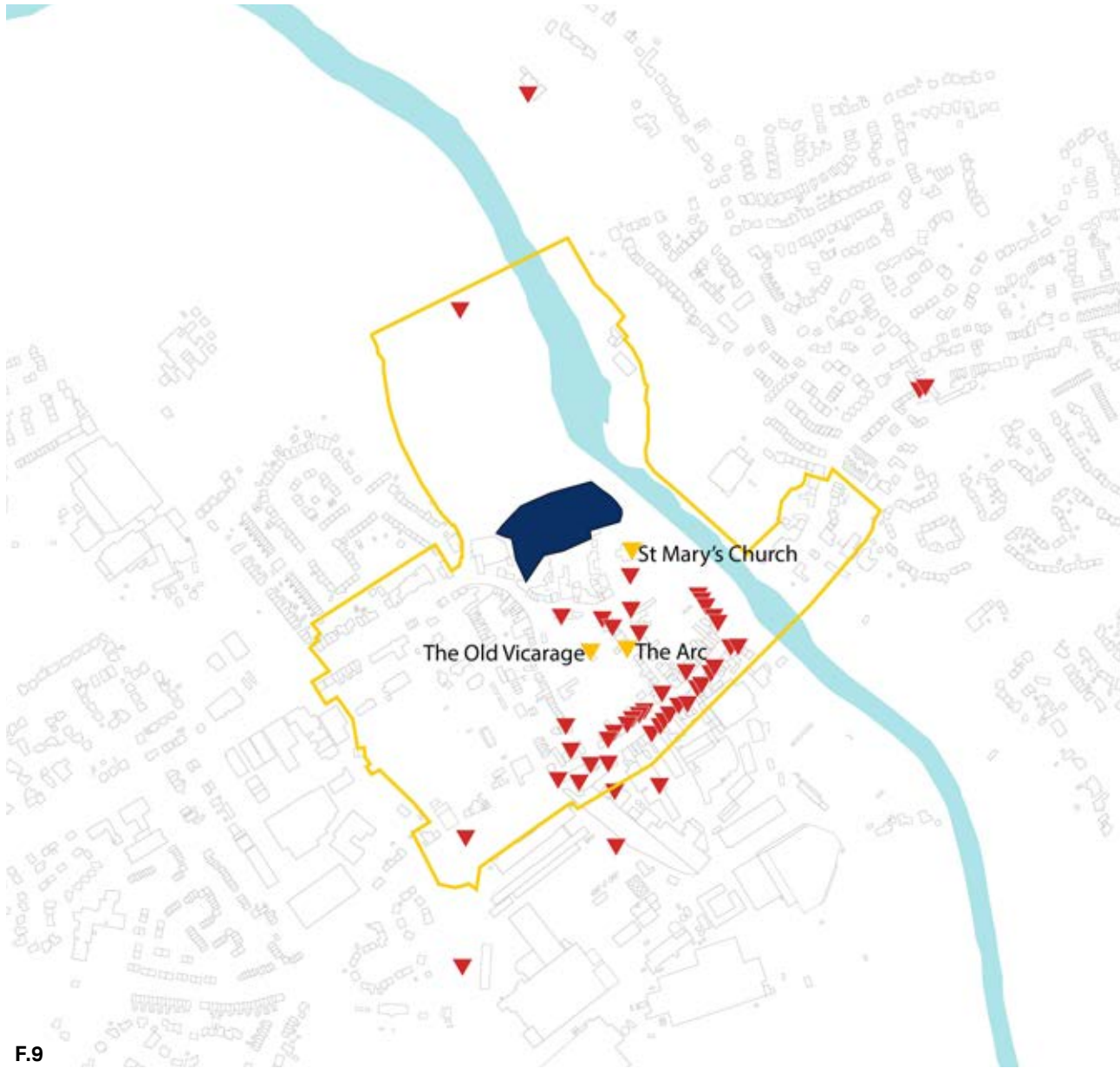
**It is not only the buildings that contribute to the historic character of the areas. Other factors that contribute include:**

- **The form of the settlement;**
- **Boundaries;**
- **Spaces between buildings;**
- **Street surfacing; and**
- **Green spaces, trees and other historic features all have a part to play.**

**Figure 07:** Wetherby Road

**Figure 08:** Mill Lane





F.9

Figure 09: Map showing conservation area, scheduled monument and listed buildings

Figure 10: The Grade II\* Listed 'The Ark' fronting Kirkgate

Figure 11: The Grade II\* Listed Old Vicarage on Church Street



F.10



F.11

## Listed buildings

Tadcaster has numerous Grade II and three Grade II\* listed buildings as set out in the table on the next page. The great majority of these are located within the conservation area.

Of those outside of the conservation area most are milestones. The others are a pair of properties on York Road, the former railways viaduct and buildings associated with John Smith's brewery

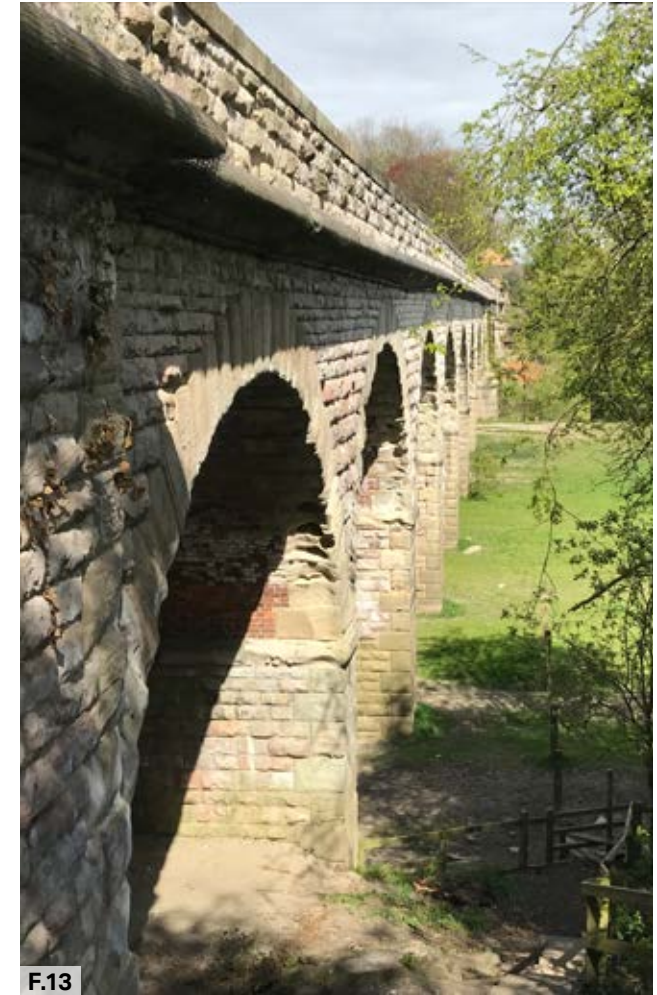
### 2.2.6 Local list

There are further positive character buildings that could be identified on a local list, although they are not considered of national importance. For example;

- St. Joseph's Catholic primary school
- Former Workhouse, St. Joseph's Street
- Riley Smith Hall, Westgate
- Former pub on Commercial Street
- The Bay Horse, Commercial Street



**Figure 12:** Chimney to John Smith's Brewery

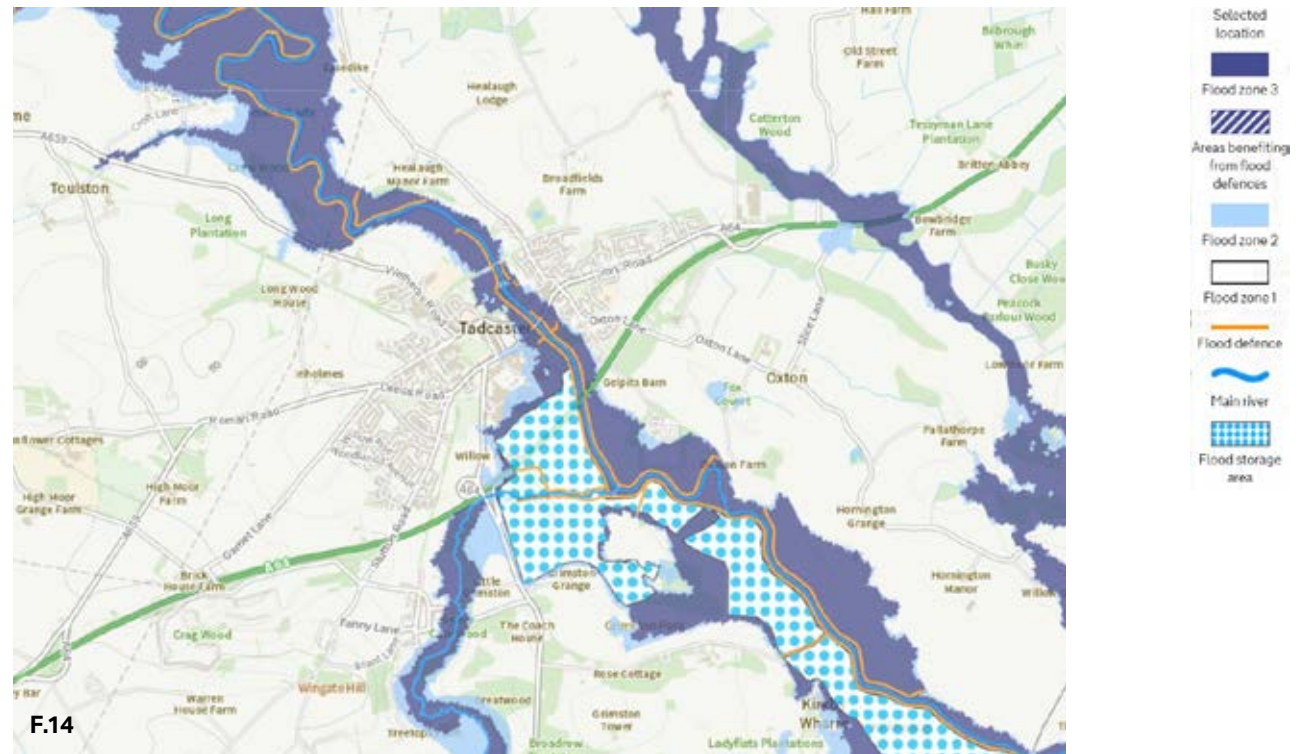


**Figure 13:** Disused Railway Viaduct over River Wharfe

Grade:	Inside Conservation Area	Outside Conservation Area
<b>Grade II Listed</b>	<p>Milestone at Junction with A162  Walls/gates/piers adjoining offices to John Smith's Brewery  Offices to John Smith's Brewery  House used as Director's office for John Smith's Brewery  Enquiries office and walls/piers/gates adjoining John Smith's Brewery  High Street (No's 1, 3, 5, 7, 9, 10, 14, 16, 18, 24, 26 and 28)  Chapel Street (No's 1 and 3)  Methodist Church and Numbers 32 and 36  The Old Brewery  The Angel and White Horse  Bridge Street (No's 1, 3, 5, 11, 13, 15, 17, 19 and 21)  Wharfe Bank Terrace (No's 1, 2, 3, 5, 7, 9 and 11)  Wharfe Bridge  Sundial (south porch of St Mary's Church)  The Old Sunday School  Kirkgate (No's 24, 32, 47 and 49)  K6 Outside Numbers 3 and 5  Barclay's Bank</p>	<p>York Road (No's 16 and 18)  Disused Railway Viaduct over River Wharfe  Milestone 1 mile east of Milestone opposite junction with Croft Lane  Milestone approx 0.5 miles from junction with Garnet Lane  Milestone approx 0.25 miles from junction with Sutton Lane  Milestone approx 80 metres west of junction with Carnet Lane  Chimney to John Smith's Brewery  Malt Tower and adjoining ranges to John Smith's Brewery</p>
<b>Grade II* Listed</b>	<p>Church of St Mary  The Ark  The Old Vicarage</p>	

## 2.3 Flood risk

Tadcaster is situated on the River Wharfe with a road bridge crossing the river in the centre of the town, connecting the two sides and communities. There are existing flood defences that were constructed in the 1980's, these comprise of both raised embankments and flood walls along both banks of the River Wharfe. There are also a number of washlands upstream and downstream of the town known as Tadcaster Ings. The Ings store water to reduce flood levels and are utilised regularly. These defences have protected the town many times from flooding over the years, more recently the frequency and severity of events have resulted in the town suffering a series of flooding events.



**Figure 14:** Extract of Environment Agency flood risk zones for planning and flood defences in Tadcaster



**Townscape & Character**

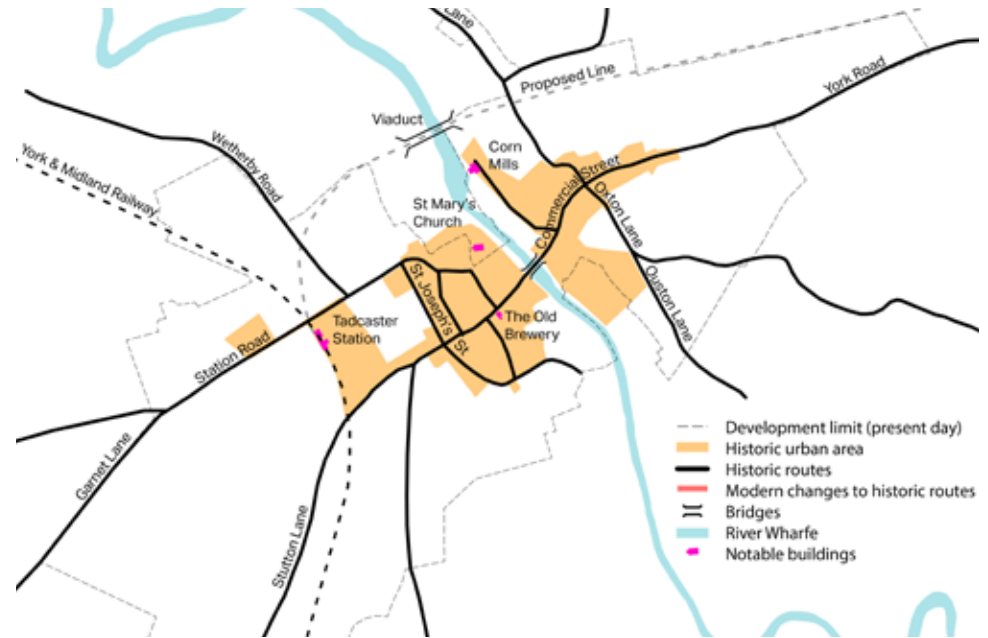
**03**

### 3. Townscape & character

This section begins with analysis of the whole town under a series of themes that help to understand the overall spatial pattern and features of the town. This informs a series of character study areas that follow. A study of some key focus areas also highlights examples of existing housing areas that have lessons for future development.

#### 3.1 Historic growth

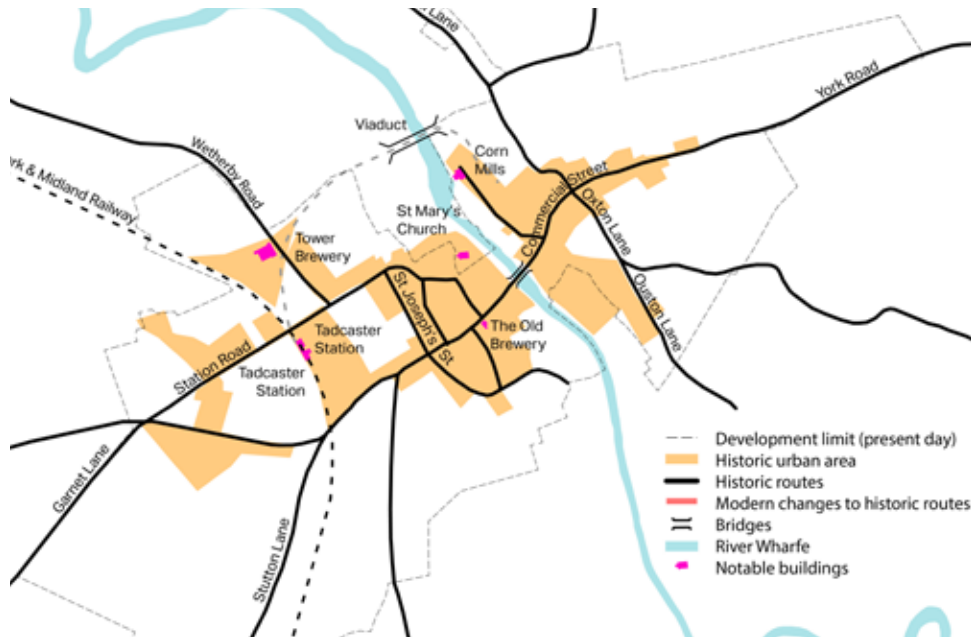
Tadcaster town has a varied built character consisting of traditional architecture from the late Middle Ages (15th century onwards), architecture associated with the established local brewing industry (1700s onwards) and more recent (post WW2, 1970/80s and post 1990s) architectural character. The historic area of the town is further designated as a Conservation Area.



#### 1849

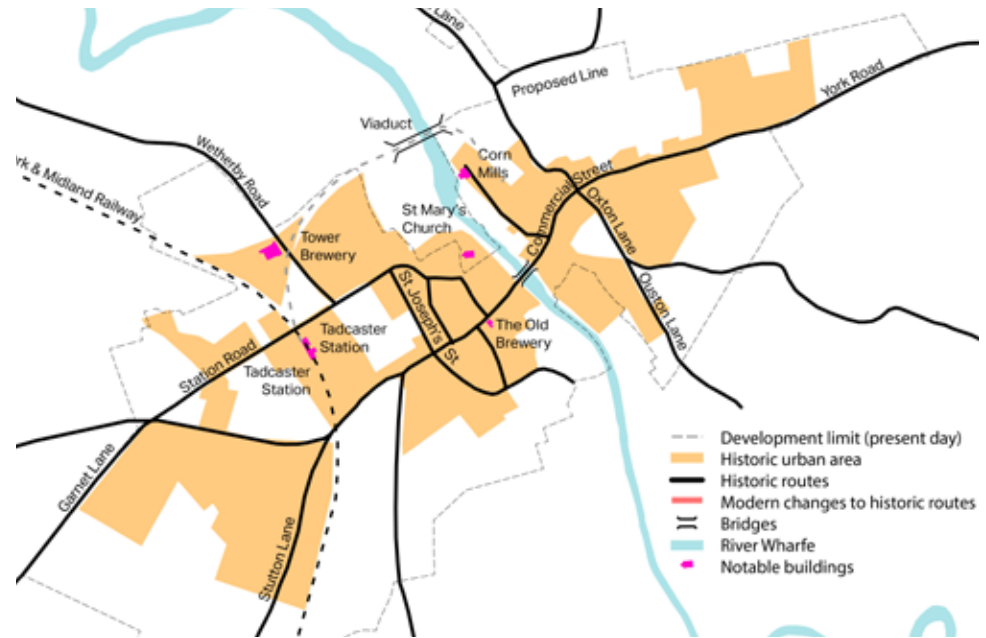
- The former Roman name for the town of Tadcaster as Calcaria. York-Leeds Road broadly follows the course of a Roman Road.
- Majority of development pattern is in a linear form with a strong frontage line along High Street / Bridge Street / Kirkgate.
- Buildings on West Gate, adjacent to St Mary's Church are laid out in an organic form with curved and irregular frontage lines.
- Tadcaster station is located outside of the main settlement area. A proposed railway branch leads north-east from Tadcaster over a viaduct crossing of the River Wharfe.
- Local industrial activity identified on the historic mapping include: a clay pit and brick kiln, Limekilns, a corn mill, a grain wharf, a railway coal depot, and a gas works.





## 1909

- A railway now crosses the River Wharfe providing rail access to the corn Mills to the east.
- Tadcaster Union Workhouse and Cemetery are introduced to the south-west of the station. Golf course and tennis courts are introduced to the south of the Cemetery.
- The proposed branch of the railway extending to the north east of the town is no longer identified on the mapping.
- Residential development grows along existing street pattern including small-scale development to the west of the town.
- Mapping shows a Malthouse adjacent to railway line and more breweries are shown. Tower Brewery is built to the north of Station Road.



## 1953

- Large-scale post-war residential development
- New planned development patterns emerging at the fringes of the town with the introduction of estate and cul-de-sac forms of development.
- Development to the south of the town extends towards the edge of the civil parish boundary.



KELCBAR HILL

TOWN CENTRE

BREWERY

OXTON HALL

YORK ROAD

LEEDS ROAD

RIVER WHARFE

A64

**F.15** Figure 15: Edited aerial photograph highlighting the built-form of Tadcaster today

0 500m



## Present day

- Development extent has extended beyond the parish boundary in to Sutton and Hazelwood CP in the south and Healaugh CP in the north in places.
- The A64 bypass has been introduced to the south of the town providing easier access between Leeds and York and reducing traffic pressure in the town centre.
- The railway and station have been removed, although some remnants of the alignment remain.
- The railway viaduct has been retained as a pedestrian connection across the River Wharfe.
- The Coors Brewery has been developed to the north west of the town making three breweries in total.
- Various infill developments and sensitive restoration projects have occurred. This together with with the limited development growth mean Tadcaster retains much of its historic charm.



**Figure 16:** The Tadcaster bridge providing the primary connection between both sides of the town via the River Wharfe

**Figure 17:** St Joseph's Catholic Church

**Figure 18:** The Riley-Smith Hall theatre, ballroom and concert venue built in 1924

## 3.2 Settlement pattern

Tadcaster has retained its traditional character within its historic core and is relatively unspoilt, despite some suburban expansion and industrial employment sites surrounding the town. The overall scale of the town is modest due to limited growth and retail/ services.

The bridge is a key link, tying east and west together. The river and riverside is a 'green-blue corridor' that both divides and unites the town in various ways (identity of the town, appreciation of nature and risk from flooding). This green area has significant potential for enhancement as a riverside park.

The town centre is focused around The High Street, Kirkgate and Chapel Street; the retail core is relatively small but remains positively compact (Bridge Street/High Street and Kirkgate/ junction of Westgate/Chapel Street) and Commercial Street east of the river (including the bus station).

The remainder of the town is predominantly residential or employment focused (the size and scale of the three brewery sites are commensurate with their importance).

### 3.2.1 Legibility & wayfinding

The relationship of the Smith's breweries and the impressive architecture is very important to the town centre and a key landmark and frontage on Bridge Street.

The viaduct to the north of the town provides a second crossing of the river and is another key landmark.

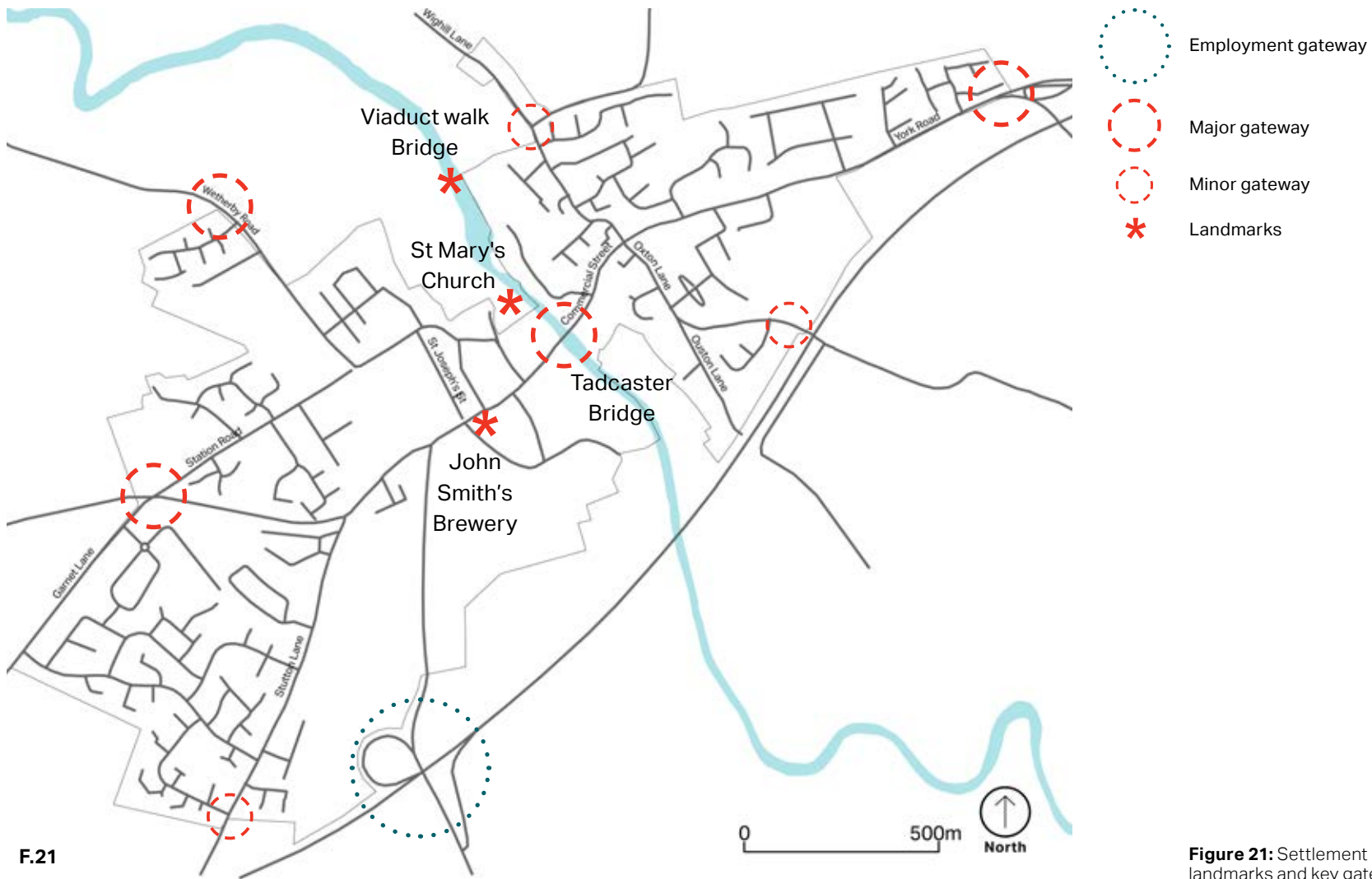
The Wharfe River, Tadcaster Bridge and Grade II\* listed St. Mary's Church are defining features of the town that help to orientate and provide key gateways between the town's eastern and western sides.

There are key gateways to the north, east and west of the town on the Wetherby, York and Leeds Road respectively.

**Figure 19:** View west on York Road

**Figure 20:** View east on Wharfe Bridge





F.21

**Figure 21:** Settlement pattern diagram showing landmarks and key gateways

**Figure 22:** View west on York Road

**Figure 23:** View east on Wharfe Bridge



### 3.2.2 Figure-ground

The figure ground plan (see plan on next page) of a town can be likened to its DNA, encapsulating its personality, both good and bad traits are apparent in the simple display of solid black (building) and white void (space).

The addition of green (open spaces) and blue (watercourses) infrastructure gives a more balanced view of the natural and man-made elements that inter-twine and make our relationship to and co-dwelling with natural processes and ecology more apparent.

This plan type is a great indicator of different characters throughout the town.

Notable characteristics:

- Large floor plates correspond to industrial and commercial uses to the south / west / NW.

- Connected buildings and frontages define a street layout in the historic core of the town.
- Varying building patterns combine to define the oldest/ longest streets and lanes leading into the town, from semi-rural, to suburban, to urban centre.
- Patterns of buildings and spaces towards the fringes of the town become looser.
- There is variety between imposed layout and seemingly organic growth in different areas.
- There are three recognisable area patterns; urban; suburban and industrial.
- The floodplain has mostly deterred development from occurring alongside the River Wharfe.



F.24

Figure 24: Figure-ground plan with green spaces

### 3.2.3 Route hierarchy

The street hierarchy and footpaths map (presented over page) also reveals strong patterns, that also help us to identify character areas according to pervading highways design. The typology of different streets across the town ranges from historic radial routes that connect to surrounding settlements or farmsteads (along which the town has grown); to the thoroughfares around town and development access loops and branches, leading dead-end cul-de-sacs that often stunt connectivity between developments (where they closed to pedestrians and cyclists).

Public rights of way follow the river's course and connect around the periphery of the town and to the countryside.

Key takeaways:

- The A659 radial routes that connect to other settlements (Wetherby Rd/York Road/Leeds Road), are well-trafficked and so key impressions of the town so development that takes place on these

is of particular significance and should respond to and enhance the traditional character of the town.

- Edge routes, lanes and paths that face or link to the countryside are likewise important as pedestrian and leisure experience routes that allow views back to the town from landscape of great attractiveness and so this impact and perception of any development on the edge is important.
- The 'face' and true expression of the historic character of the town are the main thoroughfares in the town centre and the riverside in combination that define the origin and true sense of place.

**Figure 25:** Riverside public right of way with flood defence wall leading to the former viaduct

**Figure 26:** York Road with traditional stone boundary walls and properties that are characteristic of the town's radial routes







- Public Right of Way (PROW)
- ▭ Distributor
- ▬ Radial Routes
- ▬ Inter-radial Routes
- ▬ Local Grid
- ▬ Loop
- ▬ Multi-headed Cul-de-sac
- ▬ Cul-de-sac

F.27

Figure 27: Map showing the route hierarchy in Tadcaster



F.28

### 3.2.4 Topography

The map on the next page shows the overlay of 2m contours on the figure ground plan. This reveals the relationship of buildings to land-form, the hills and valleys of the settlement, and how they have helped shaped the town in and its expansion. Tadcaster is historically a river valley settlement that has grown up on the shallower slopes of the river valley to the west, expanding up Kelcbar Hill and also expanded up the hillside to the east (Commercial Street), broadly on the route of the historic Roman Road.

Notable characteristics:

- Industrial development sits close to the river traditionally (Sam Smiths brewery) and later on developed alongside the now-closed train station (Coors brewery).

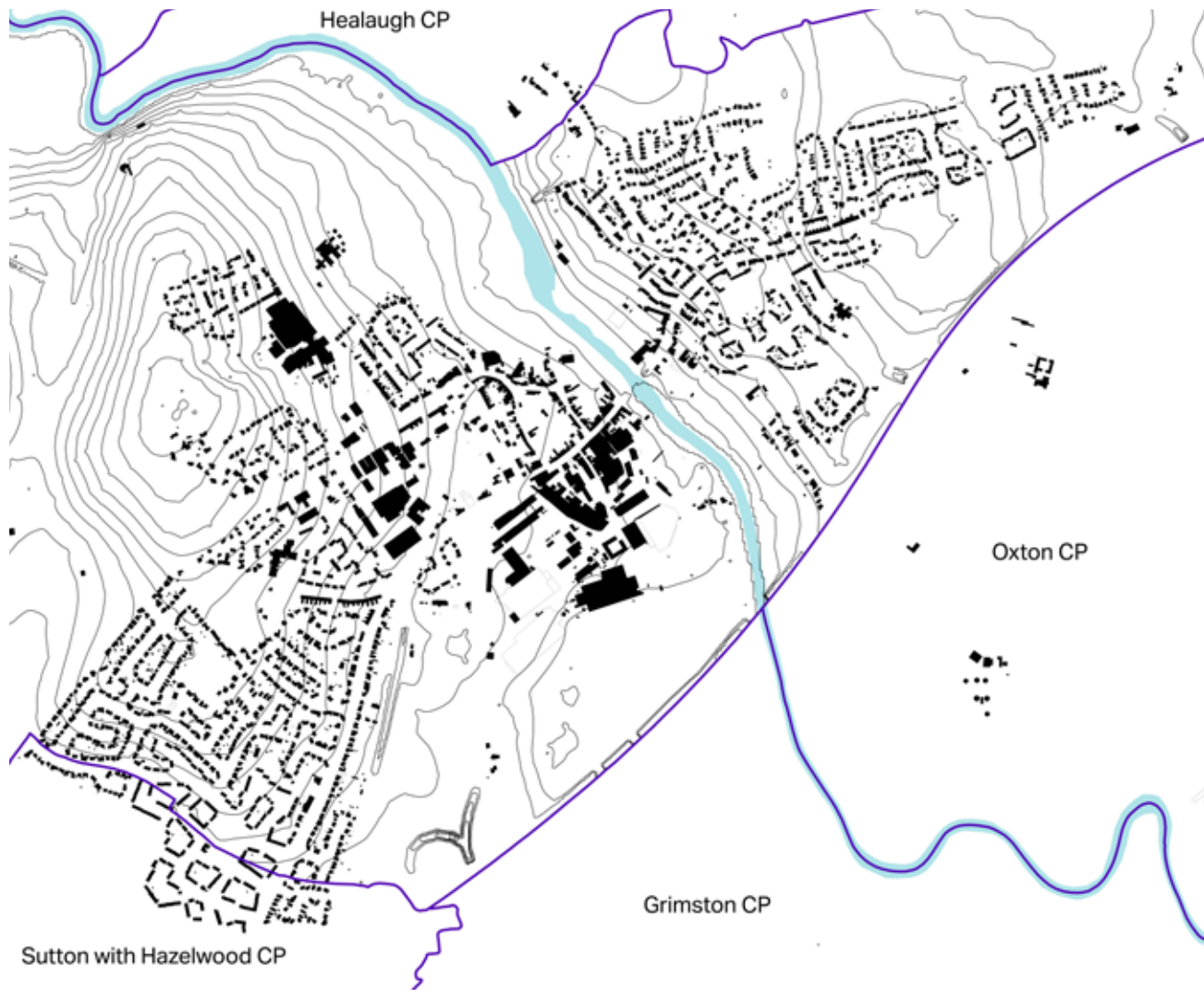
- To the west, the historic core sits on the relatively flat levels beside the river, putting it at some risk of flooding.
- To the east of the river, due to the steeper slope, settlement hugged commercial street and focused higher up around the junction with Oxton Lane (leading to Oxton Hall) at the top of the valley slope.
- Kelcbar hill is a key features of the topography. Residential development has crept up Inholmes Lane / Kelcbar Close nearly to the top of the hill but not beyond. This area is potentially visually sensitive due to its elevation.
- Development hugs the former alignment of the Roman Road closely as it extends eastwards, cresting another shallow hill.



F.29

**Figure 28:** View up Edgerton Drive on Kelcbar Hill

**Figure 29:** View from hillside on Commercial Street towards John Smith's brewery / town centre



- Parish boundaries
- Buildings
- 2m Contours
- River Wharfe

**F.30** | **Figure 30:** Map showing the heritage assets in

### 3.2.5 Recreation & open space

There are a variety of green open spaces in Tadcaster. These include; local amenity spaces and play areas, allotments, the cemetery, sports pitches and recreation grounds and the inaccessible former vicarage gardens. Although not shown on the plan, the adjacent photograph shows the green corridor that follows the course of the river, including floodplain, which has further potential to function as a linear park that brings the town together and promotes healthy lifestyles. It already has several well-used, publicly accessible routes which could be extended and enhanced further.



**F.33**

**Figure 33:**  
West bank of the River Wharfe and St. Mary's Church



**F.34**

**Figure 34:**  
The River Wharfe corridor is a large green corridor and recreational asset in the centre of the town



**F.35** | **Figure 35:** Map showing green open spaces

### 3.2.6 Neighbourhood structure

Tadcaster has a strong focus on the compact town centre which contains most of the shopping and commercial uses to the west of the river. The supermarket is located to the east of the river in the town centre.

Employment is principally located to the west of the river (including the 3 breweries).

Suburban residential communities extend furthest to the east along York Road, south-west between Garnet Lane and Stutton Lane. There are several community centres, including the Barn in the town centre.

Schools are located on Station Road, Wetherby Road and Sandfield Terrace.

#### Community Facilities & services

Tadcaster has a variety of important community facilities including;

- The swimming pool

- The library
- Several churches
- The leisure centre
- The Ark
- The Barn
- Boys Sunday School
- Kelcbar Community Centre
- Rosemary House Community Centre
- Calcaria House Community Centre
- Police and fire stations

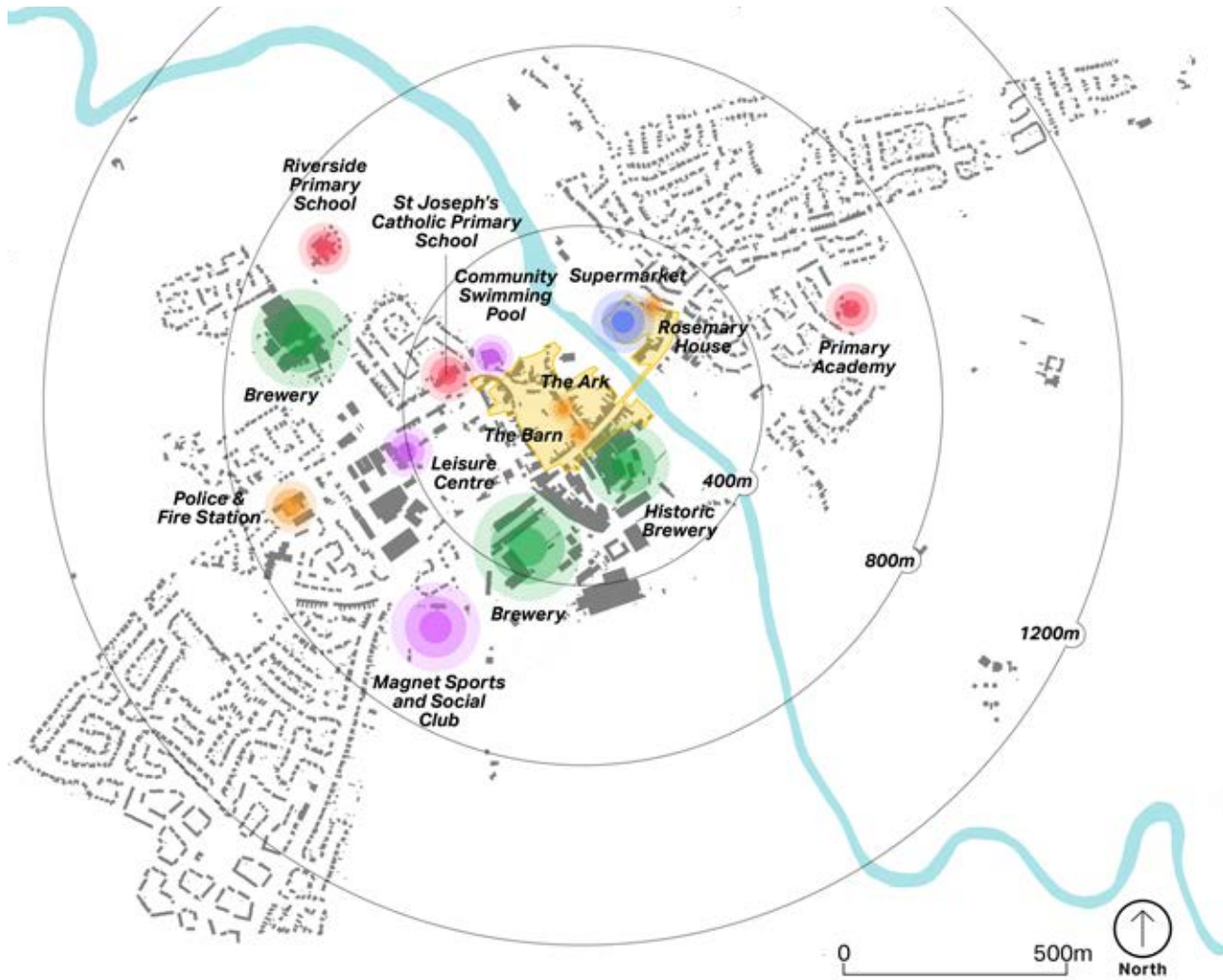
#### Walk bands & accessibility

The majority of the town's residential areas are within 1200m of the centre of the settlement as the crow flies. This is a convenient walking distance of approximately 15minutes, with variation for local topography.

**Figure 36:** Tadcaster Community Swimming Pool

**Figure 37:** Tadcaster Enterprise Park





- Shopping & Commercial Centre
- Breweries
- Civic / community services
- Schools
- Sports / leisure centre
- Supermarket

**F.38** | **Figure 38:** Map showing neighbourhood structure

### 3.3 Character areas





A key purpose of this Design Code is to help generate authentic, locally distinctive and high-quality design responses to the town context and its landscape setting.

From the setting of the River Wharfe to the edge of the settlement this includes a range of contexts from town centre, to urban terraces, to suburban homes, to semi-rural edges and lanes.

We have defined simple character areas that help understand the overall pattern of the settlement to which some of the design codes and guidance can be applied.

Within these broad areas further defining characteristics are relevant including; historic period and building types; landscape character; intensity of use and predominant urban form (layout & density), along with topography and features.

Potential sites requires a different design response to set the tone for regeneration, infill or expansion in keeping with Tadcaster dependent on location and area character.

C1	Historic core / town centre	
C2	Tadcaster East (suburban)	
C3	Tadcaster West (suburban)	
	Industrial (non-residential)	

#### 3.3.1 Focus areas

Due to the broad nature of the suburban character areas (Tadcaster East and Tadcaster West), particular focus areas have been described in more detail in the following analysis. These Focus Area studies help to set out some of the more positive characteristics displayed by particular areas or periods of development.

#### Focus Area A: Oxton Lane / Grange

**Road** - post-war estate development in the context of the historic Oxton Lane (and Oxton Hall).

#### Focus Area B: Turnpike Road/ Eastern Gateway

- late C20 development in the context of Slip Inn Farm at the edge of town

#### Focus Area C: Kelcbar Hill / Western / Northern Gateway

- variety of smaller-scale developments and lanes rising up the hillside

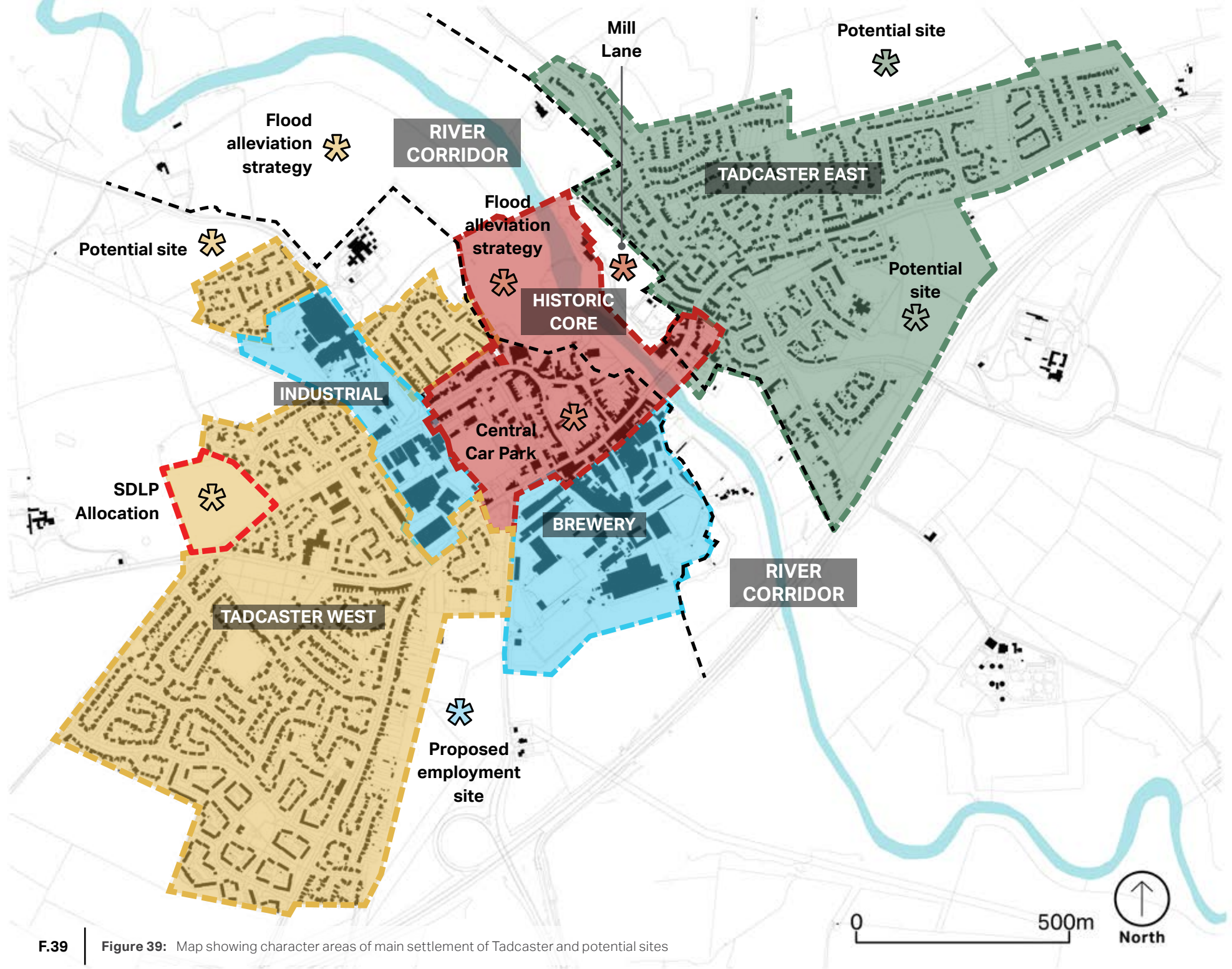
#### Focus Area D: C20 suburban housing estates

- phased expansion of the town to the south-west with a variety of architecture.

#### 3.3.2 Historic Lanes / radial routes

The historic lanes / radial routes through Tadcaster are a key part of its character. Streets as much as areas are relevant to how we perceive character. These routes typically lead/ led to other settlements or historically important destinations such as grand houses or quarries). They also display characteristics that are important.





F.39 | Figure 39: Map showing character areas of main settlement of Tadcaster and potential sites

# C1

## Historic core / town centre

### Historic core/town centre

The historic core straddles the River Wharfe which flows north to south through Tadcaster. A conservation area was designated in 1973 by the East-Riding. The draft conservation area appraisal in Feb 2021 found no reason to alter the boundary which matches this character area. As such the conservation area appraisal final version should be referred to when considering this character area.

The historic core of Tadcaster remains largely intact (barring the town centre car park) and the building quality is generally high. The public realm has been improved in key places but there remain some empty buildings, degrading frontages and surface level car parks and yards to address.

The John Smith brewery and its Grade II listed gateway acts as a key landmark within the historic core (and town), marking the start of the High Street to the west and forming a strong gateway to the town centre. The C18 Samuel Smith Brewery fronts on to the high street, adding to the

quality of the historic street-scene.

The River Wharfe, its Grade II listed stone bridge and the former railway viaduct are the second most visually and spatially dominant features within and outside the conservation area. The banks of the river form important open spaces in the town.

The areas along both banks of the River Wharfe, including the parish church and Mill Lane lie in Flood Zone 3, which is characterised as susceptible to regular, severe flooding. There is a potential flood alleviation scheme on land to the north of the parish church, to the west of the river.

The historic core has retained a lot of its integrity in scale, layout and retained building form, justifying its designation. As such it represents a small historic market town in Yorkshire. There has been some conversion and restoration of buildings.

The High Street and Kirkgate stand out with their taller and grander former residential buildings principally in hand made brick, than say, Chapel Street which retain smaller stone built 'cottage style' buildings.



F.40

Figure 40: St Mary's Church



F.41

Figure 41: Historic frontages along Bridge Street



F.42



F.44



F.43



F.45

**Figure 42:** Samuel Smith's 'The Old Brewery' building fronting Bridge Street

**Figure 43:** Historic retail frontages along Westgate

**Figure 44:** Bridge Street looking down towards the John Smith's Brewery complex

**Figure 45:** Historic townhouses fronting Kirkgate

Category	Townscape characteristics
<b>Land use</b>	<ul style="list-style-type: none"> <li>Town centre, mixed-use; retail, commercial; industry, community facilities, residential; institutional; leisure and recreation.</li> </ul>
<b>Layout &amp; Density</b>	<ul style="list-style-type: none"> <li>Organic pattern of near continuous building frontage, set to back of pavement following key historic routes. More loosely defined frontages on the periphery. Building coverage has decreased over time and been replaced by parking within blocks.</li> </ul>
<b>Access</b>	<ul style="list-style-type: none"> <li>High street, secondary commercial streets and small alleyways survive between buildings leading to dwellings, outbuildings and yards within rear plots.</li> </ul>
<b>Landscaping</b>	<ul style="list-style-type: none"> <li>Limited space for tree planting in the town centre; planters and flower displays; trees and green infrastructure along river corridor.</li> </ul>
<b>Topography</b>	<ul style="list-style-type: none"> <li>Most of the area lies within flood zone 1 but there are areas that are prone to extreme flooding (e.g. Bridge Street). Commercial Street rises more steeply to the east, land to the west of the river is on a gentle slope.</li> </ul>
<b>Landmarks &amp; Features</b>	<ul style="list-style-type: none"> <li>Brewery chimneys, two brewery frontages (Samuel Smiths &amp; John Smiths), River wharfe; stone bridge and viaduct.</li> </ul>
<b>Views</b>	<ul style="list-style-type: none"> <li>Dynamic views along individual streets (Kirkgate, High Street, Bridge Street, Chapel Street, and Westgate) and glimpsed and framed views of the two Smith Breweries (John Smith and Samuel Smith). Also views along river corridor.</li> </ul>

**Table 03:** Village characteristics table

Figure 46: Sample analysis map thumbnails for the historic core / town centre



F.46

Factors	Appearance characteristics
<b>Building types</b>	<ul style="list-style-type: none"> <li>Built forms vary across the conservation area from small-scale, eighteenth-century former workers' cottages to grand, three-storey brick residences with servants' quarters in lofts.</li> </ul>
<b>Building scale</b>	<ul style="list-style-type: none"> <li>Buildings tend to diminish in size away from the centre and range from grand, three-storey, eighteenth- and nineteenth century buildings on the High Street and Kirkgate to more modest, two-storey, eighteenth- and nineteenth-century.</li> </ul>
<b>Materials</b>	<ul style="list-style-type: none"> <li>Red-brick (traditionally hand-made); magnesian limestone; render; slate roofs or pantiles.</li> </ul>
<b>Boundaries</b>	<ul style="list-style-type: none"> <li>Generally, limestone rubble walls with occasional limestone ashlar and late C19 / early C20 brick.</li> </ul>
<b>Thresholds</b>	<ul style="list-style-type: none"> <li>Most buildings front directly onto the pavement within the conservation area.</li> </ul>
<b>Roofscape</b>	<ul style="list-style-type: none"> <li>Pitched roofs - pitches mostly in line with street; occasional gable ends presented to the street creates variety in the roofscape; chimneys punctuate skyline.</li> </ul>
<b>Public realm</b>	<ul style="list-style-type: none"> <li>Improvements in key places; some widening of pavements in commercial areas with narrow pavements found generally.</li> </ul>

### 3.3.3 Traditional Materials

Mostly there are a mix of materials (see table) creating a degree of interest and contrast between buildings. Some streets (such as Chapel Street) conform more closely and are mainly stone built.

- Rough-hewn magnesian limestone ashlar is the main building material of the smaller eighteenth- and early nineteenth-century cottages and terraces, with the best examples on Chapel Street.
- Later cottages use 'machine sawn' magnesian limestone ashlar of a more uniform size (see images).
- The eighteenth- and nineteenth-century buildings on the High Street and Kirkgate are predominantly brick.
- Traditional timber frame shopfronts survive.

**Table 04:** Appearance characteristics table

### 3.3.4 Boundaries and streetscape

Boundary walls are a key feature of Tadcaster and can be seen throughout the conservation area. Generally, they are of limestone rubble construction with occasional limestone ashlar and late nineteenth- century/early-twentieth-century brick. Low walls are used to the front of plots, sometimes with railings.

### 3.3.5 Heritage Assets

There are 39 grade II listed and 3 grade II\* listed in this area. There are further non-designated heritage assets, character buildings and landmarks, including:

- St. Joseph's Catholic primary school
- Former Workhouse, St. Joseph's Street
- Riley Smith Hall, Westgate
- Former pub on Commercial Street
- The Bay Horse, Commercial Street

### 3.3.6 Issues & Opportunities:

- There are some derelict areas, car parks and empty or underused buildings (including some listed buildings) - these could be brought back into use or improved environmentally;
- Potential flood alleviation scheme as part of a new riverside park;
- Inauthentic, replacement shopfronts can undermine character - modern architecture may be more appropriate than inauthentic pastiche in places;
- Public realm requires further expansion of improvements and the pedestrianisation of Kirkgate is a key aspiration; and
- There are limited Infill development sites and all are located within a sensitive historic context, however there is great advantage to concentrating sensitive housing growth around the centre to support its vitality and viability.



F.47

Figure 47: Tadcaster War Memorial from Chapel Street



F.48

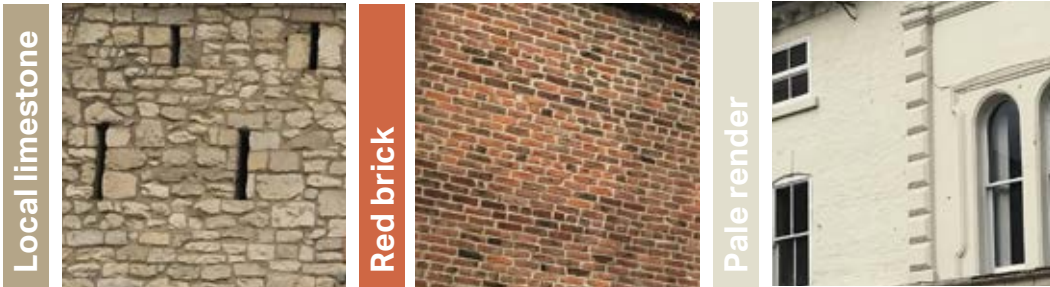
Figure 48: Grade II\* Listed The Old Vicarage

# Key characteristics

## Colours and materiality



## Façade



## Roofing



### PRIMARY ROOF TYPE

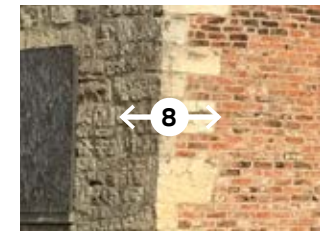
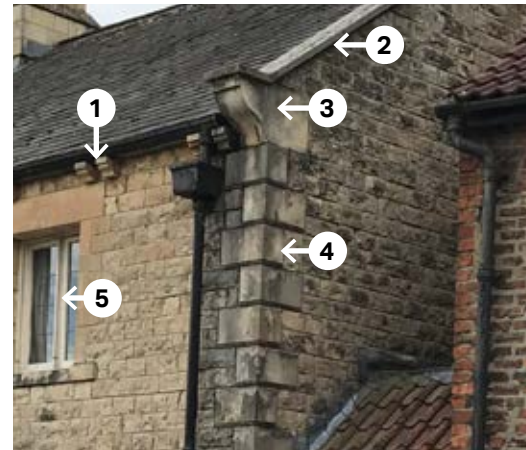
Red pantile roofing is very typical of the Neighbourhood Plan Area and beyond.



### SECONDARY ROOF TYPE

Grey slate roofing although less common makes a notable contribution to local character.

## Stone detailing



1. Dentil masonry adorning roofline
2. Limestone coping
3. Limestone corbel / kneeler
4. Chamfered quoining
5. Limestone mullions
6. Decorative limestone band courses
7. Splayed keystone window header
8. Many buildings have front and side elevations made from different materials (red brick and limestone)



## Doorways



1. Glazed door with ornate wooden surrounds
2. Wooden panelled door with arched window and keystone surround
3. Wooden panelled door with stone surround and pointed pediment

## Windows



1. Curved bow window with paned sash windows and timber framing
2. Bay window with paned sash windows and timber framing
3. Arched window with paned sash windows and timber framing
4. Oblong paned sash window and timber framing

## Boundary treatments



1. Black cast iron railings on low-rise red brick wall with limestone capping
2. High-rise limestone boundary wall
3. High-rise red brick boundary wall with limestone capping

## Historic Core / Town Centre Design Recommendations:

- The town centre car park is a low quality environment at the heart of the historic town and requires improvement or redevelopment. An alternative site for parking is under consideration. Robin Hood's Yard site is used for residents parking and can also be improved in quality.
- The site off Mill Lane should be sensitive to the riverside setting and conservation area context.
- Westgate, Kirkgate, the High Street and Commercial Street are the 'face' of the town centre / conservation area and require traditional, high quality and characterful design approach to boundaries and frontages to upkeep and enhance the strong sense of place and heritage value.
- The commonly used materials of red brick and local limestone should be the default options for new materials for infill development. The repetition or combination of these should be judged on the context and streetscene that development forms a part of.
- Limestone wall boundaries to the front of properties and developments should be reinstated on property

boundaries where development is set back from the pavement; sometimes sawn ashlar or red brick may be appropriate depending on the context.

- The set-back of development from the street will generally be shorter than the suburban areas of Tadcaster (from 0 – 2m) to respect the tighter historic grain and retain the urban character in the centre and interface of entrances and shopfronts with the street.
- There are some small-scale infill opportunities which should respect adjacent buildings, building lines and street characteristics where these are positive, or apply broader principles of character from within the historic core.
- Townhouses, terraces and linked cottages form a mostly continuous, or joined-up (by masonry walls) streetscene. Infill development should continue this pattern. Special buildings which relate to historic, community or work function may stand out from this backdrop due to importance.
- Landmark and character buildings should be retained and respected by new development, and may inform design concepts for new development where appropriate, although inauthentic pastiche is generally discouraged.

- Derelict areas, car parks and yards should be provided with suitable boundaries and made suitably attractive with robust materials, landscaping and upkeep, particularly where facing the public realm.
- Empty or underused buildings (including some listed buildings) should be explored for community use or housing and refurbishment to avoid degradation of the built form and boarded up windows and doors that undermine the street scene.
- Authentic (form, scale and materials) traditional replacement shopfronts are encouraged, modern (e.g. UPVC) imitations are discouraged and well-designed modern alternatives may be more suitable.
- Character features and details should be retained and restored to maintain the richness and variety of detailing within the area, as appropriate to the period of development.
- Land to the north of Tadcaster Castle scheduled monument is within the river floodplain and visually sensitive (views from bridge and viaduct), as well as archaeologically sensitive (potential Roman origins). This should form part of a flood alleviation scheme.



## Tadcaster East

### 3.4 Tadcaster East

This area extends from the town, east of the river and up the hillside (see figure 39). It has is a key focus at cross-roads with Oxton Lane & Wighill Lane. The area extends further east along York Road mainly, up to Slip Inn Farm.

Although a linear settlement pattern historically, some suburban housing estate development has occurred in between. These developments are accessed from York Road, Oxton Lane and Wighill Lane, the key historic structure of the area. Selective demolitions, infill development and ribbon development have also occurred on these main routes.

The area retains a mixed development character overall including; Georgian cottages; Victorian terraces; inter-war estates, post-war estates and late C20 estates.

Category	Townscape characteristics
<b>Land use</b>	Predominantly residential neighbourhood; some neighbourhood amenities including primary school and allotments.
<b>Layout &amp; Density</b>	Area is linear in form and focused around three historic routes (York Road, Oxton Lane and Wighill Lane); several medium density housing estates infill in a relatively narrow band along and behind the York Road principally.
<b>Main streets/lanes</b>	These three key corridors connect this eastern neighbourhood to the town centre. These are the primary movement corridors for all modes, connecting to quieter residential streets. There is a cross-roads where these join Commercial Street.
<b>Open space &amp; landscaping</b>	Triangular green space between Oxton Lane and Ouston Lane; several grass verges; mature trees along some streets; a green wedge is retained between Park Drive & prospect drive; development areas tend to be well-screened from the wider landscape by bands of tree planting.
<b>Topography</b>	The area is relatively flat, sitting atop the hillside that rises sharply from the river valley; Wighill Lane and Oxton Lane broadly run along the top of the hillside and York Road continues to rise and then dip down, over a very shallow hill.
<b>Listed &amp; character buildings</b>	Listed buildings; 16, 18 York road (Georgian townhouse and attached cottage). The Grange is located off Ouston Lane; Brick houses located off Wighill Lane; Slip Inn Farm at the eastern gateway to York Road.
<b>Views</b>	View to Church from the top of Rosemary Row; View to brewery tower from top of commercial street.

### 3.4.1 Key corridors

#### York Road

This main road is a key street and gateway to the east of the town / town centre which follows the general alignment of the Roman Road to York. It transitions from urban, to suburban, to semi-rural at the fringes. Along the way are some other semi-rural 'fringe features' such as allotments (elements from previous town edges) as the town has expanded.



F.52

Figure 52: Map highlighting York Road



F.49

Figure 49: Eastern gateway starts as a semi-rural green corridor with the Slip Inn Farmstead on the left marking York Road



F.50

Figure 50: Moving along York Road into the town, the development is increasingly suburban and residential



F.51

Figure 51: Towards the cross-roads on York Road development becomes more urban (terraces) and increases in density

## Wighill Lane

Semi-rural Lane running north from York Road out towards Healough Manor. Older buildings include simple vernacular working buildings and cottages (including the character building Brick Houses). A variety of infill development retains a rural village lane feel with bends in the lane likely corresponding to fields patterns an topography. Mid-height stone walls are characteristic along the property boundary / lane edge.



F.53

Figure 53: Map highlighting Wighill Lane / Oxtun Lane



F.54

Figure 54: Wighill Lane

## Oxtun Lane / Oxtun Lane

Semi-rural lane running south to Oxtun Hall. A mix of building types, mostly 21st century with some retained character buildings such as The Grange. Stone walls define the front boundaries and help to integrate a variety of development. The green where the lane meets Oxtun Lane (now a dead-end, since the building of the bypass) contributes to a rural village feel, as do the allotments behind York Road and by Oxtun Lane. Oxtun Lane is a tranquil semi-rural lane with has no pavements with green verges and stone boundary walls, creating an intimate scale.



F.55

Figure 55: Oxtun Lane



F.56

Figure 56: Oxtun Lane

# A

## Focus Area: Oxton Lane / Grange Road

### 3.4.2 Oxton Lane / Grange Road

A post-war, planned development with green verges and tree planting possibly inspired by the garden city movement (although without the intricate arts and crafts style architecture). The development integrates with the historic Oxton Lane and has a degree of street hierarchy, blending from suburban development to a semi-rural character at the edge.



F.57

Figure 57: Satellite image of Oxton Lane and Grange Road

Factors	Appearance characteristics
<b>Building types</b>	<ul style="list-style-type: none"> <li>The built-form is mostly made up of mid-20th century semi-detached dwellings but with some detached dwellings and bungalows located along Ouston Ln and Oxton Dr towards the rear of the estate.</li> </ul>
<b>Building scale</b>	<ul style="list-style-type: none"> <li>Buildings tend to diminish in size slightly from York Rd with most development being 2-storeys. Development towards to the rear of the area between Oxton Ln and Ouston Ln is predominantly made up of 1-storey and 1.5-storey bungalows.</li> </ul>
<b>Materials</b>	<ul style="list-style-type: none"> <li>Red brick; Pale render; slate roofs or pantiles.</li> </ul>
<b>Boundaries</b>	<ul style="list-style-type: none"> <li>Generally, limestone rubble walls and hedgerows with some late C20 brick walls.</li> </ul>
<b>Thresholds</b>	<ul style="list-style-type: none"> <li>Dwellings have significant setbacks in the form of front gardens and / or driveways.</li> </ul>
<b>Roofscape</b>	<ul style="list-style-type: none"> <li>Red pantile pitched roofs throughout most of the area apart from the more recent development between Oxton Ln and Ouston Ln which are mostly characterised by welsh slate gable end.</li> </ul>
<b>Public realm</b>	<ul style="list-style-type: none"> <li>Generally double paved streets. Several streets characterised by grass verges / buffers as well as some mature trees lining streets such as Grange Road.</li> </ul>

### Positives:

- Stone walls help to embed the development frontage into its context;
- Larger set backs on Ouston Lane for front gardens help soften building appearance and respect the character of Ouston Lane;
- Hedgerows define plot boundaries giving a very green feel to streets and paths and protecting privacy; Trees planted in street verges create an avenue;
- Grange Avenue has mature tree planting, providing an attractive environment for pedestrians;
- There is some variety of street types and spaces (although basic hard landscape treatment);
- Ginnels in terraces allow direct access to back yards for moving bins; and
- Brick flat-arches over windows are one of few concessions to traditional building detailing.

### Negatives:

- Simple red-brick building forms are relatively basic, hipped-roofs are fairly shallow;
- Hedgerows and boundaries removed to accommodate some parking on plot undermine the streetscape; and
- Green spaces have been lost in order to provide additional parking areas.



**Figure 59:** Red brick semi-detached dwellings along Ouston Ln



**Figure 58:** Grange Rd characterised by typical housing estate layout with central green space / parking area and grass verges and trees



**Figure 60:** Grass verges and mature trees lining the access of Grange Rd

# B

## Focus Area: Turnpike Road

### 3.4.3 Turnpike Road

A private housing development (circa 1980's – 1990's). A modest suburban development to the eastern end of Tadcaster at the town's gateway. The development uses traditional materials and a variety of houses types which, in some ways, aim to respond to the character of Slip Inn Farm and farmhouse which are local character buildings opposite the site.



F.61

Figure 61: Satellite image of the Turnpike Road development

Factors	Appearance characteristics
<b>Building types</b>	<ul style="list-style-type: none"> <li>Late 20th century development of detached dwellings.</li> </ul>
<b>Building scale</b>	<ul style="list-style-type: none"> <li>All 2-storey development.</li> </ul>
<b>Materials</b>	<ul style="list-style-type: none"> <li>Limestone; red pantile roofs.</li> </ul>
<b>Boundaries</b>	<ul style="list-style-type: none"> <li>Little to no boundary treatments between dwellings. Occasional limestone walls and planting.</li> </ul>
<b>Thresholds</b>	<ul style="list-style-type: none"> <li>Dwellings have significant setbacks in the form of front gardens and / or driveways.</li> </ul>
<b>Roofscape</b>	<ul style="list-style-type: none"> <li>All dwellings have red pantile gable end roofs.</li> </ul>
<b>Public realm</b>	<ul style="list-style-type: none"> <li>Most streets are double paved and some have grass verges / buffers separating pavement from the road.</li> </ul>



### Positives:

- Takes some material and character cues from Slip in Farm; uses traditional materials including; limestone and pantiles which relate to Tadcaster;
- Varied building detailing includes; stone rear-boundary walls; porches; chimneys; characteristic bay windows;
- Small front gardens and informal planting on plot;
- Frontage to York Road set back behind mature hedgerows creates a green wildlife corridor;
- Mature woodland planting to east and north screens development from the wider landscape; and
- The shared-surface 'homezones' without separate pavements feel most successful as streets (e.g. east-field close).

### Negatives:

- Set back and density of planting on the site frontage means development has little presence, so pedestrians may feel less safe walking along York Road;
- Modern detached house types; closely positioned; with integral garages sometimes jar with the aesthetic;
- Front of plot parking is very exposed without boundaries/hedgerows (narrow garages may not be usable); and
- No front boundary treatment (e.g. low walls / rails / fences or hedgerows).



**Figure 62:** Detached dwellings along Turnpike Rd



**Figure 63:** The Slip Inn Farm fronting York Rd and adjacent to the Turnpike Rd development

# Tadcaster East: Key characteristics

## Façade



## Roofing



## Boundary treatments



- 1. Mature hedgerow
- 2. Medium-rise limestone boundary wall

## Tadcaster East Design Recommendations:

- Listed buildings such as 16-18 York Road, Fircroft and Oxton Hall are heritage assets that require high-quality development in their context.
- Character buildings such as Slip in Farm, Brick Houses and The Grange should be retained and respected by new development, and may inform design concepts where appropriate.
- The set-back of development and landscaping on main routes in and out of the town should relate to the transition from urban to suburban to rural.
- Where buildings are set back from the street, traditional stone wall boundaries are crucial to integrating development and adding a sense of place and quality to the street scene on the main streets and lanes (York Road / Oxton Lane / Wighill Lane). Hedgerows are also important to strengthen green corridors and connection to the landscape.

- Development related to the main streets/lanes should use predominantly traditional or characteristic materials (limestone, brick and pantile)
- Development on main routes or in proximity to heritage assets should use vernacular or characteristic building forms that integrate with the changing character of this main route through the town - ranging from semi-rural, to suburban, to urban location.
- The town's landscape context to north-east is open and potentially visually sensitive. Development on the periphery that can be seen from the landscape may need to be screened with buffer planting or consideration should be given to colours and materials that blend with the landscape.
- In the more well-screened landscape context to the south-east (e.g. Hargarth Field) hedgerow and fields patterns are key to character. Noise and air-quality around the bypass is a key consideration.

- Tadcaster Mere is within 500m of the east of Tadcaster and the SSSI impact zone is of relevance to any potential development sites to the south-east of York Road.



## Tadcaster West

### 3.5 Tadcaster West

This area extends north-west and south-west of central Tadcaster, gently rising up towards Kelcbar Hill (see figure 39). It consists of a mix of commercial, municipal and residential uses. The Leeds Rd (a later connection) and Station Rd (a possible roman road alignment) form the main routes into and out of the centre. There are two key junctions at; Station Rd/Wetherby Ln and Leeds Rd/Stutton Rd. Where Garnet Ln, Station Rd and Leeds Rd converge marks the western gateway to the town.

As a historically compact settlement little development has occurred to the west. A small cluster existed on Stutton Ln and expanded along Station Rd in Victorian times. Most buildings west of the centre are from Victorian times onwards and associated with the station and breweries. Commercial developments are sandwiched between Leeds Rd and Station Rd, with some later housing. Station Rd and Leeds Rd joined across the railway by the end of C20.

Category	Townscape characteristics
<b>Land use</b>	Residential neighbourhoods, industrial areas (including Coors brewery), municipal facilities (fire, police); leisure centre and riverside primary school; extra care retirement apartments.
<b>Layout &amp; Density</b>	Dual linear core along Leeds and Station Road; mazy C20 suburban extension to the south-west; smaller scale lane and cul-de-sac developments around base of Kelcbar Hill.
<b>Main streets/lanes</b>	Leeds Road and Station Road are dual routes into/out of the town centre; the other residential roads (Wetherby, north) and lanes (Garnett Lane, Stutton Road, south-west) branch from these.
<b>Open space &amp; landscaping</b>	Tadcaster cemetery is a large peaceful, burial space; Magnet sports ground and allotments.
<b>Topography</b>	Land gently rising to the west from the river; rising more steeply around Kelcbar Hill.
<b>Listed &amp; character buildings</b>	There are no listed buildings in Tadcaster West; character buildings include Victorian cemetery gatehouse.
<b>Views</b>	Views to surrounding landscape from public rights of way at the top of Inholmes Lane/Kelcbar Close; views across river corridor landscape from Wetherby Road beyond Riverside primary; View to landscape from western gateway; view over allotments/sports ground to landscape from Leeds Road.

Further, suburban housing estate development, have fanned-out, accessed via Station Rd, Stutton Ln, Garnett Rd and Wetherby Rd. The residential development periods include; Victorian terraces; expansive inter-war / post-war housing estates and late C20 housing infill.

**Figure 64:** Map highlighting Station Road/ Garnet Lane and Leeds Road

**Figure 65:** Leeds Road terraces

**Figure 66:** Station Road Avenue tree planting



**F.64**

### 3.5.1 Key corridors

#### Leeds Road

A mixed-character street with a variety of developments and residential styles. Victorian terraces facing late C20 bungalows. The cemetery and faces the extra care retirement apartments that replaced the union workhouse. A mix of hedgerow, low brick walls and some sections of stone wall define property boundaries with some close-board fencing. This is a less locally distinctive street in many respects than the York Road to the east.



**F.65**

#### Station Road

A long straight road, possibly following the alignment of a Roman Road, leads to the West Gate of the town centre. Mostly developed in the latter half of C20 and onwards. Victorian terraces at the centre lead to wide, planned, tree lined street giving access to the town's various 'fringe features' including; enterprise park; school; fire station; open space police station; recycling centre and Coors/Tower breweries. Further west, stone walls line the street with breaks for access to smaller residential clusters at the foot of Kelcbar hill.



**F.66**

## Stutton Lane

Historic lane leading to Stutton village. Developed throughout C20 (mainly 1910-1980). A mix of brick wall, stone wall and hedgerow boundaries define properties. House types vary from terraces to semi-detached to detached houses mainly, heading out of town.

## Wetherby Road

Red brick and slate roof Victorian terraces (opposite the modern brewery) are not characteristic of Tadcaster's Georgian origins and typical materials palette but form an upstanding piece of town. Development in the 1970's on Kelcbar Hill is uncharacteristic in terms of layout, materials and scale and somewhat visible from the wider landscape. Some later infill developments have returned to the common town palette of stone and pantile which help to knit this into the overall fabric of the town, as do the stone wall boundaries around the school.



F.68



F.69



F.67

**Figure 67:** Map highlighting Wetherby Road / Stutton Road

**Figure 68:** Stutton Road, mixed periods of development

**Figure 69:** Wetherby Road terraces



## Focus Area: Kelcbar Hill / Western Gateway

### 3.5.2 Hillside / Western Gateway

Varied periods of smaller scale historic lanes and hillside plot development using a variety of materials, some characteristic of Tadcaster (stone and pantile). Aligned to hillside and often integrated within the landscape by existing woodland and set within narrower field boundaries aligned to topography. The area has a key focus around confluence of routes at the western gateway (Leeds Rd/Station Rd/Garnett Ln).



F.70

Figure 70: Satellite image of Kelcbar Hill / Western Gateway

Factors	Appearance characteristics
<b>Building types</b>	<ul style="list-style-type: none"><li>Varied periods of development of both bungalows and detached dwellings.</li></ul>
<b>Building scale</b>	<ul style="list-style-type: none"><li>Mostly 2-storey dwellings but some 1 and 1.5 storey bungalows.</li></ul>
<b>Materials</b>	<ul style="list-style-type: none"><li>Red brick; limestone; Red pantile roof; Welsh slate roof.</li></ul>
<b>Boundaries</b>	<ul style="list-style-type: none"><li>Limestone walls; hedgerows.</li></ul>
<b>Thresholds</b>	<ul style="list-style-type: none"><li>Dwellings have significant setbacks in the form of front gardens and / or driveways.</li></ul>
<b>Roofscape</b>	<ul style="list-style-type: none"><li>Mixture of red pantile gable ends and welsh slate gable ends.</li></ul>
<b>Public realm</b>	<ul style="list-style-type: none"><li>Most streets double paved apart from Inholmes Lane which is mostly lined by grass verges and hedgerow.</li></ul>

## Key takeaways

- Developments located around the foot of Kelcbar hill and rising up the hillside; includes winding lanes and cul-de-sacs (Inholmes Lane, Edgerton Drive and Cedar Drive).
- Smaller scale field patterns and woodland setting create smaller housing clusters nestled within remnant woodland and field boundary vegetation.
- Based on historic hillside lanes (Inholmes lane) and some newer cul-de-sacs (Edgerton Drive / Cedar Drive).
- Proximity to Edgerton Lodge (unlisted character building) and Inholmes (house, also unlisted) to the west gives a more private estate character (reflected in the newer development names ending in Drive - historically indicating a private lane).
- Development up from Edgerton Lodge on Inholmes Lane is a meandering hillside lane with a semi-rural feel at the summit and views out to the landscape.
- On some main routes the access strategy (single or multi-cul-de-sac)

leads to inward facing developments sometimes with inadequate boundary treatments (Cedar Drive to Leeds Road for example).

- The station road allocated site relates to this area and conditions of development.
- Stone walls are important as part of the estate character of Inholmes and also main streets into Tadcaster (Station Road / Leeds Road).



Figure 71: Inholmes Lane



Figure 72: Station Road



Figure 73: Station Road



# D

## Focus Area: C20 suburban housing estate estates (1920-1980)

### 3.5.3 C20 suburban housing estate extension (1920-1980)

Successive housing developments from the inter-war, postwar and later half of C20 between Roman Ridge (Garnett Lane and Stutton Lane) from 1920-1980. Generally non-locally distinctive mass-produced suburban housing with a variety of layout principles according to era.



F.74

Figure 74: Satellite image of development between Garnett Lane and Stutton Lane

Factors	Appearance characteristics
<b>Building types</b>	<ul style="list-style-type: none"> <li>Varied periods of development between 1920-1980 including bungalows, semi-detached, and detached dwellings.</li> </ul>
<b>Building scale</b>	<ul style="list-style-type: none"> <li>Predominantly 2-storey dwellings but substantial amount of 1 and 1.5 storey bungalows also.</li> </ul>
<b>Materials</b>	<ul style="list-style-type: none"> <li>Red brick; Welsh slate roof; Red pantile roof.</li> </ul>
<b>Boundaries</b>	<ul style="list-style-type: none"> <li>Low-rise red brick walls; hedgerows.</li> </ul>
<b>Thresholds</b>	<ul style="list-style-type: none"> <li>Dwellings have significant setbacks in the form of front gardens and / or driveways.</li> </ul>
<b>Roofscape</b>	<ul style="list-style-type: none"> <li>Mix of roof types due to mix of development periods. Ranges between welsh slate pitched roofs, welsh slate pitched roofs, and red pantile gable ends.</li> </ul>
<b>Public realm</b>	<ul style="list-style-type: none"> <li>All streets double paved some of which have grass verges / buffers separating the pavement from the road.</li> </ul>

## Key takeaways

- Some relatively flat land first developed, then extending south down lower hill slopes.
- Based on broader field pattern initially – ease of development, few remnant features.
- Development does address historic lanes with building frontages of changing character.
- Standardised house-types and materials by phases creates some variety.
- Semi-formal layout with semi-detached houses in the early phase, looping streets with detached bungalow/ chalet style homes in mid-phase; experimental courtyard terraces in a broad parkland setting at Windmill Drive later (Radburn-esque).
- Lack of legibility in areas due to curvilinear streets, T-junctions and cul-de-sacs rather than obvious onward connections for pedestrians and cyclists.
- Mono-type estates; semi-detached, detached and terraced homes according to phases.
- Privet hedge boundaries matured to create good front boundaries in first phase; low brick walls help define the second phase; the third phase has extensive front lawns and verges (ownership not always clear).
- Parkland setting in third phase is attractive but likely has a significant maintenance requirement.



F.75

**Figure 75:** 1950's detached and semi-detached dwellings along Marlborough Drive



F.76

**Figure 76:** Chalet-style 1-1.5-storey bungalows along Woodlands Avenue



F.77

**Figure 77:** Semi-detached bungalows setback with large front gardens on Woodlands View

# Tadcaster West: Key characteristics

## Façade



## Boundary treatments



## Roofing



1. Mature hedgerow
2. Medium-rise limestone boundary wall
3. Low-rise red brick boundary wall

## Tadcaster West Design Recommendations:

- Leeds Road, Station Road, Wetherby Road and Stutton Road are main routes and key approaches. As the 'face' of the western approaches to the town centre they require a traditional, locally distinctive design approach to boundaries and facades to improve this character;
- The local palette of limestone and red pantile (occasionally red brick) should be reinforced on all key corridors into the town from the west with Victorian red brick in specific areas in keeping with local context;
- Substantial stone wall boundaries to the front of properties and developments should be reinstated wherever possible on key approaches to the town;
- The set-back of development and landscaping on main routes in and out of the town should relate to the transition from urban to suburban to semi-rural on the periphery;

- Greenbelt should prevent further expansion around the periphery of Tadcaster West and Garnet Lane should remain a semi-rural green lane;
- Land to the west of Whetherby road on Kelcbar Hill is potentially sensitive visually due to the hillside location and proximity to the river floodplain. It is also green belt and not recommended for development. Any future development must repair this edge and integrate very sensitively with the landscape and townscape of Tadcaster to form a low-key semi-rural northern gateway;
- There is an existing allocated site on Station Road at the western gateway which is more well-screened (externally) by existing woodland but also holds a prominent position at the western gateway that should be developed carefully to present a characteristic arrival to Tadcaster using the local palette of materials (limestone and red pantile) (see also section 5 design code for further detail);

- The site frontage and boundary to the allocated site should continue the stone wall treatment of the neighbouring Inholmes estate;
- There are limited small scale infill opportunities including the SHLAA potential site opposite the police station which should consider loss of mature vegetation and biodiversity;
- Estate regeneration could be considered on some older and poorer quality housing estates (particularly fronting main routes) but even the less characteristic homes have settled and are now better blended with mature landscape. This would be a longer term consideration but is likely undesirable to residents and developers; and
- Characterful buildings such as Inholmes, Inholmes Lodge, The Jackdaw (circa 1970 but stone built) and cemetery gatehouse should be retained and respected by new development, and may inform design concepts where appropriate.



**Design guidance & codes**

**04**

# 4. Design guidance & codes

**This section sets out principles and guidelines that must influence the design of all new development within the Neighbourhood Plan Area. Where possible, local examples are used to illustrate the issues. Where these images are not available, best practice examples from elsewhere are used.**

## 4.1 Introduction

The design guidance in this section addresses any major or minor development within the development boundary of Tadcaster (where it is encouraged on brownfield sites to support the town centre) or any development outside of the development boundary within the neighbourhood plan area that may come

forward, although this is likely to be opposed. The restrictions, constraints and issues outlined in section 2 of this report (including flood risk, green belt and SSSI) must be considered first. Likewise, the design recommendations in Section 3 relating to character areas must also be addressed alongside this guidance. Key major sites, such as Land North of Station Road, Land at Mill Lane and Central Area Car Park are referred to in the design codes in Section 5 also.

## 4.2 General design principles

The following is a more generalised list of aspirational design principles for all new development in Tadcaster:

1. Preserve the special character and heritage of the town;
2. Promote high quality design of all new residential, town centre and community use development;

3. Focus new housing development in and around the fringes of the town centre;
4. Steer development away from the periphery of the town (unless allocated);
5. Consolidate and support the town centre as a focus for community / mixed-use;
6. Bring buildings back into use and unlock development potential of town centre sites;
7. Reinforce the need for continuing streetscape improvements in the historic core / town centre;
8. Instil confidence in the design proposals coming forward to people of the town and meet stakeholder aspirations;
9. Support the delivery of sustainable development on all site allocations; and
10. Include the delivery of flood defences as part of a new riverside park.

## 4.3 Design code for urban infill sites

Infill development is generally smaller scale development (1 - 10 homes) in an existing urban/ developed context and may include new construction on vacant or derelict plots, repurposing declining areas, or even modifying existing building structures so they can serve a new use. The following design principles apply to any urban infill development that may come forward within the plan period, via applications on allocated or non-allocated sites within the urban area of Tadcaster.

### 4.3.1 Aim

To promote context sensitive infill housing of a high quality, including high quality affordable housing, on gap sites within the town to reinforce the economy of the town centre

### 4.3.2 Priority

To build housing in the heart of the town to consolidate the vitality of the town centre.

### **Design principles to ensure that infill development is of a high-quality and contributes to existing streetscapes and character areas:**

- **Building entrances should address the street with a main access and main fenestration. Corner buildings should address both streets with fenestration and the main entrance on the main street in the hierarchy;**
- **The building line should reflect the street and be set back no more than a maximum of 2m from adjacent buildings; where buildings are set back from the pavement a masonry boundary wall (usually low) with or without vertical iron railings should define the plot and link up to adjacent buildings;**
- **Materials should reflect local character and harmonise with adjacent buildings with red brick or stone masonry construction and red pantile or grey slate roofs (mix);**
- **Building fenestration and pattern should be in keeping with the predominant positive buildings character on the street or harmonise with adjacent buildings of good character;**
- **Building façade design should respect the horizontal rhythm of plots and building subdivisions on the street in order to harmonise and maintain visual interest;**
- **Building scale and position on plot should help to define and enclose the space within the street corridor or square to an appropriate degree based on the existing street section and level of enclosure.**
- **Buildings heights should vary from 2 – 3 storeys depending on adjacent plots. A variable eaves line and ridgeline is allowed to create interest but variation between adjacent buildings should be a maximum of 0.5 storeys in general;**
- **Rear or side plot boundaries which face public spaces must be masonry walls (material to match adjacent plots);**
- **Cycle parking - should be provided for residential and business properties;**
- **Access and storage for bins should be provided and bin stores should be designed to be integrated with plot boundaries; and**
- **Secure ginnels may be considered for terraced buildings with four or more units in order that any bicycle and bin storage to the rear can be easily brought to the streetside.**

Default building line set to back of pavement or matched to adjacent plots, maximum set back of 2m from adjacent buildings (e.g. to allow for bus stop in front)

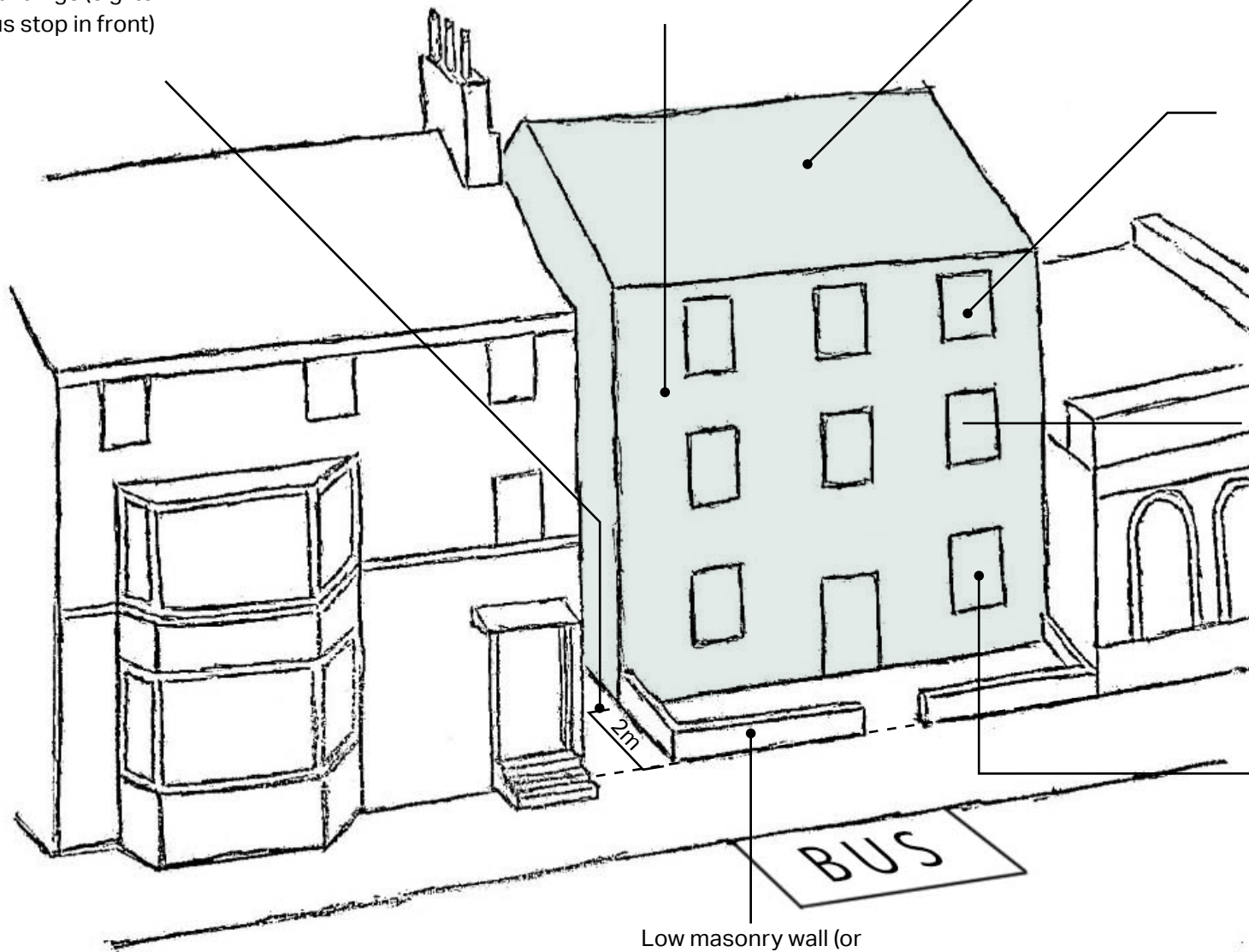
Buildings height between 2 - 3 storeys to integrate with neighbouring buildings

Red pantile or grey slate roofs to harmonise or add to mix dependent on street-scene

Facade design should harmonise and maintain visual interest in response to street scene character.

Fenestration proportions (e.g. ratio of solid to void and verticality) to respond to existing character buildings such as Georgian townhouses

Main access and fenestration to face main street, with consideration of porches / bays if appropriate to use



Low masonry wall (or with vertical railings) defines plot boundary if setback required

F.78

Figure 78: Indicative infill development diagram

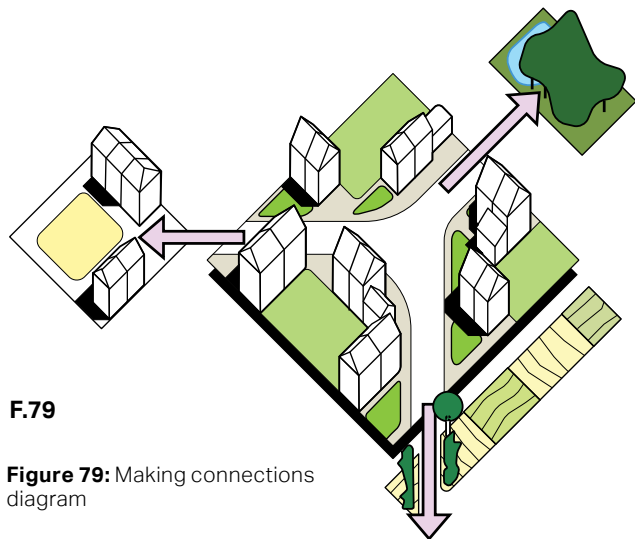


## 4.4 Design code for major development / urban extensions

The priority for Tadcaster is to develop housing on brownfield sites around the town centre that support the vitality of the town centre. However, it is prudent of the neighbourhood plan group to include design principles for major development on peripheral sites as a backstop position.

For example, unsupported sites listed as Potential sites from the SDC SHLAA.

These design principles apply to any major housing development that may to come forward within the plan period, via allocation or appeal, within or outside the Tadcaster town boundary.



F.79

**Figure 79:** Making connections diagram

### Movement & Connectivity

- The character of all streets or lanes providing access to the development, or a frontage to the site, must have a suitable design response from new development. Homes should be outward facing to existing routes but may be set back behind green verges for screening or habitat connectivity;
- A hierarchy of local street types (primary, secondary, tertiary and edge street) should be provided, suitable to the context and complexity of the development, to provide variations in character throughout development;
- Pedestrian and cycle desire lines should form the basis for the movement network, around which vehicle traffic can be managed;
- Vehicle access and internal junction geometry design must be kept to a minimum as large vehicle turning spaces are incompatible with encouraging walking and cycling within and around development. Direct, safe routes for pedestrians across junctions must be provided. Junctions should place pedestrians first in terms of scale of carriageway to cross (ie. use smaller corner radii) and also by allowing gentle ramps up to raised tables so pedestrians and wheelchairs can cross at grade;
- The development should be designed for a range of speeds from walking pace (e.g. home zones/shared-surface lanes) up to up to 20mph where suitable using short street segments (less than 70m);
- Horizontal speed restraint measures are allowed, including visual narrowing of carriageway, on street parking bays, landscaping and sets across the carriageway in a semi-rural fashion;
- Street trees are to be included within adequate verges, alongside the carriageway, on plot or in open spaces and street lighting and other infrastructure must be designed in combination;
- Footways may be on one-side of the street only in specific circumstances, for example where a lane provides access to a handful of dwellings only;
- Cycling routes should generally proved on off-carriageway routes within the green infrastructure network where possible and connect to key destinations/ onward routes; and
- Design in a general consideration of dynamic views that consider street-scenes, building arrangements and landscape unfolding from a pedestrian perspective in and out of the site.



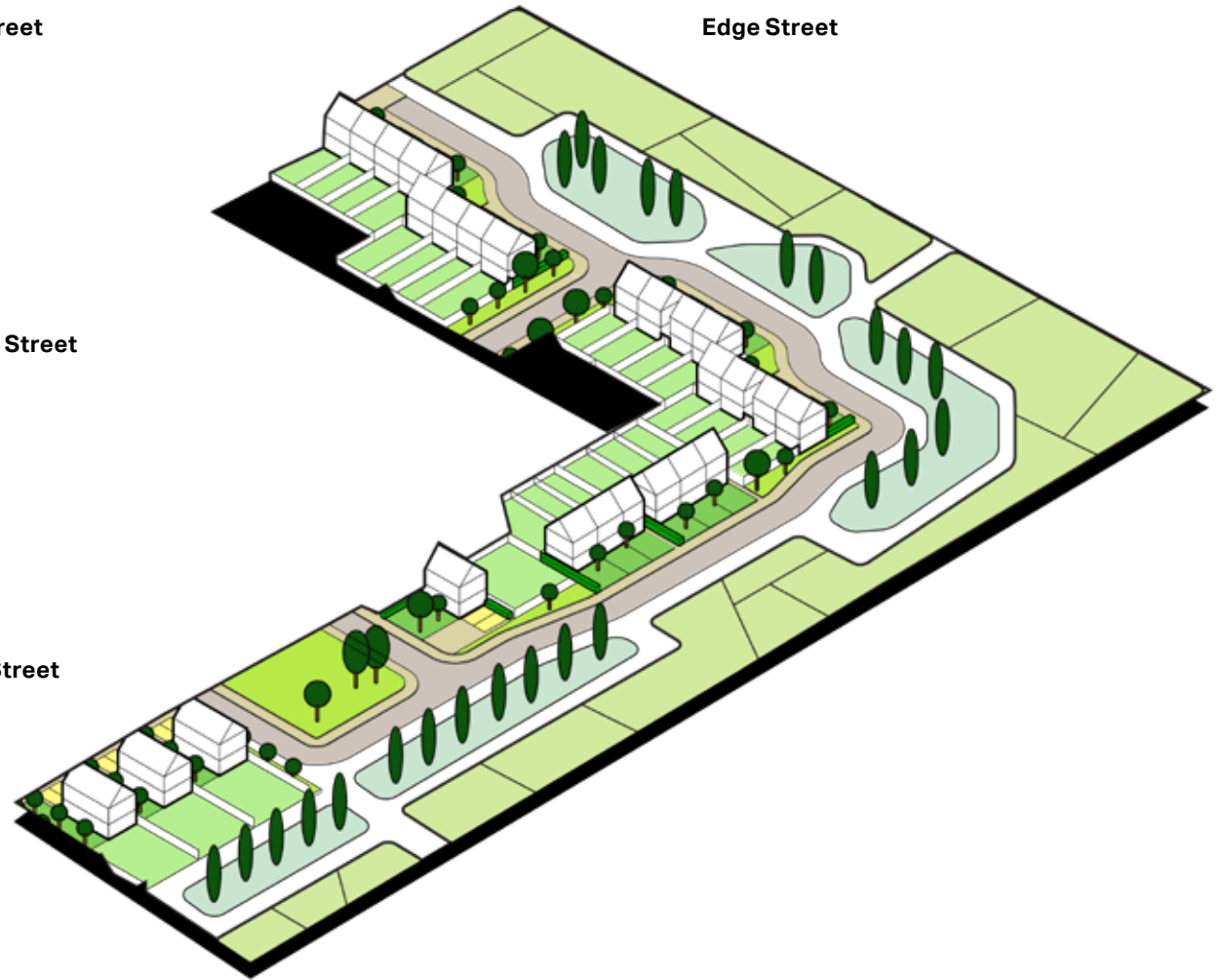
Primary Street



Secondary Street



F.80



Edge Street

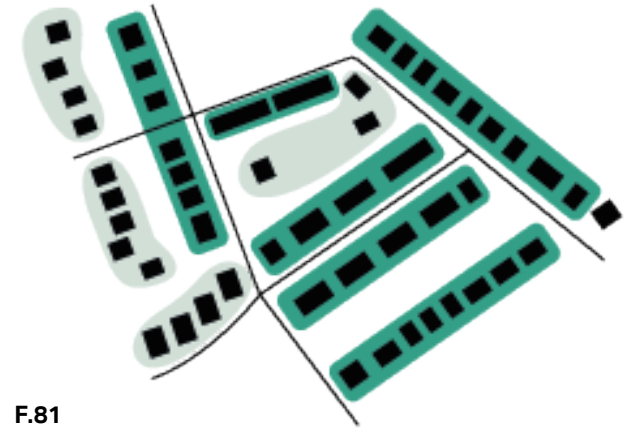
Tertiary Street

Figure 80: Indicative street hierarchy models

## Local character response

- Where developments relate to main radial streets and lanes within the town then rubble stone wall boundaries should define the site frontage (subject to any existing hedgerows to retain);
- Plot boundary treatments must be provided to define public and private space and add to the definition and sense of ownership along streets. Boundaries of low stone walls should be used on primary access streets (closed board fence should not face public spaces);
- Materials and style of home should be responsive to local character in conception with consideration for local styles and the materials (e.g. limestone masonry construction and red pantile roofs within the town) at the edge of town;
- Innovation in materials and construction techniques may be appropriate if achieving considerable sustainability credentials but must be attractive and sympathetic to local styles;
- Sympathetic styles of architecture should be presented when facing existing traditional properties of the town and semi-rural edges;

- Dwellings should respect the topography and drainage pattern of the site, respecting the landform and aligning routes and paths on comfortable gradients where possible;
- The development pattern should be laid out informally overall but with some gradation from more formal to less formal spaces (and/or changes in density) to create variety and interest;
- New development should have a distinct gradation of streets character and spatial enclosures punctuated with open spaces or set piece buildings or landscape arrangement at key gateways and intersections to create memorable places. Standardised suburban street designs without local character are not appropriate; and
- Streets and paths should use rolled stone in any tarmac areas to improve visual appeal of public spaces; large expanses of tarmac undermine the public realm of many new developments.
- Requirements for parking should not undermine the attractiveness of the streetscene. Cars should be integrated sensitively into the street scene and screened from view by buildings and property boundaries.



F.81

**Figure 81:** Development pattern - formal (dark green) and less formal.



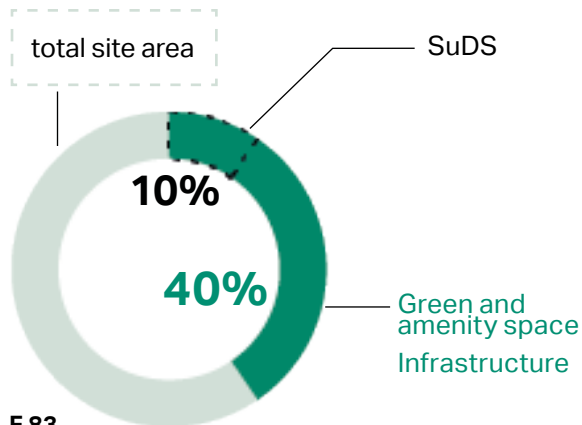
F.82

**Figure 82:** Local material samples (pantiles and limestone)

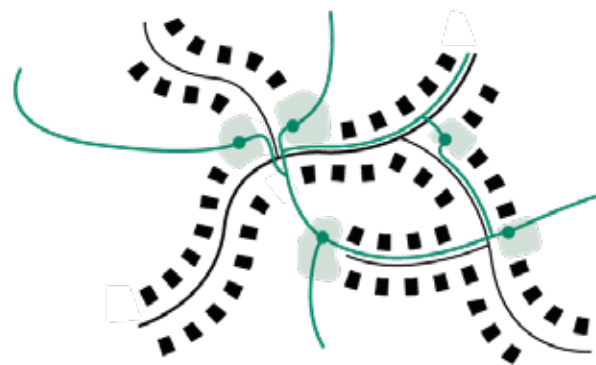
## Landscape and Green infrastructure

- Retain existing trees and hedgerows of good quality on the site within verges and in open spaces within the layout of new development;
- Include generous offsets to boundary vegetation such as hedgerows and provide additional planting on sensitive landscape edges – screen planting may be required on some sites and should be designed in conjunction with results of any landscape and visual impact surveys;
- Circa 40% of the site should be retained as green infrastructure, 10% of which may be required for SuDS detention or attenuation features dependent on drainage character;

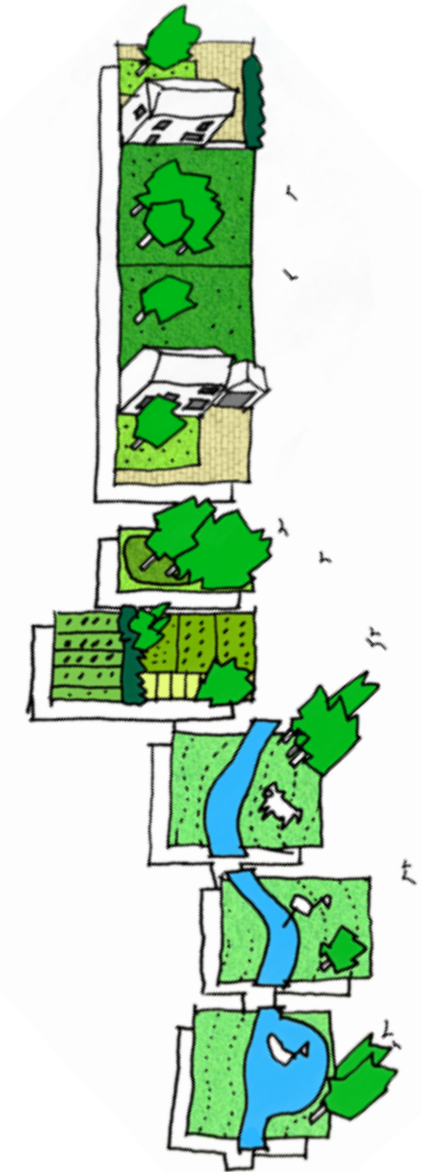
- The location of SuDS features will naturally be determined by topography (working towards the lower end of the site) and must be outside of the key flood risk areas;
- The appearance, massing and scale of development should be filtered and broken-up by green infrastructure such as open spaces, trees, hedges and planting;
- Green infrastructure elements should be combined to form a multi-functional green network. Existing and new planting should knit together within this network at a range of scales, with minimal breaks to create connected habitats and routes for wildlife.



**F.83**  
**Figure 83:** Green infrastructure to consider



**F.84**  
**Figure 84:** Linking green infrastructure plan diagram



**F.85**  
**Figure 85:** Linking and consolidating multi-functional green and blue networks sketch

## Community and neighbourhood

- Developments must be designed in a neighbourly way to respect existing development plots or buildings adjacent to the site. They must provide suitable offsets to prevent overlooking of private space and be of a comparable density and scale so as not to overwhelm existing buildings or compromise existing good character;
- Developments should aim to provide a public outdoor spaces that allow positive interaction and communities to form (whilst discouraging blind spots for anti-social behaviour);
- New community services and facilities should be designed as a focus of new development or combined to improve existing sites where they can be more widely accessible to the town;
- Benches and seating should be located to allow appreciation of views and at convenient places for people to stop and rest or stop for a chat; and
- Front gardens should have well defined boundaries that allow a sense of ownership, personalisation and the opportunity for a degree of interaction between neighbours;
- Play areas should be overlooked and use natural materials and equipment.



F.86

**Figure 86:** Natural play, allotments and community orchards can bring the community together in shared activities



F.87

**Figure 87:** Open spaces overlooked by homes will encourage use and perception of safety



F.88

**Figure 88:** Characteristic stone boundaries help settle development of different styles into the town



F.89

**Figure 89:** Add caption

## Built-form and knitting into the town

- The density of peripheral sites should typically be circa 30 DpH, in line with local plan requirements and include a mix of housing types and a variety of different tenures, including up to 40% affordable housing.
- Buildings heights may vary from 1.5 – 2 storeys with a varied roofscape that helps to break up the appearance of development on the edge of the town or where it is visible from key approaches or countryside rights of way;
- Plots should utilise front boundary treatments of hedgerows or low masonry walls to the public areas (especially rear or side boundaries which must not be close board fence);
- Building position and landscape features should define the streets and spaces between them, not the other way around (in other words, buildings should not all be offset in a standardised way from the edge of the street that they face); and
- Building types that refer to the semi-rural working building vernacular such as cottages; farmsteads; barns and rural housing clusters should inform house type design.



**Figure 90:** Indiacitive building precedents



**F.91**

**Figure 91:** Farmsteads have been retained and converted within the urban area as the town has expanded. Retaining this heritage within the townscape provides some richness and can also inspire new development forms in more rural areas

A photograph of a stone building with a large green circular overlay containing the text 'Site design codes 05'. The building is made of dark stone and has several windows. In the foreground, there is a stone wall and a paved path. A bus is visible on the right side of the path. The scene is outdoors with trees and a clear sky.

**Site design codes**

**05**

# 5. Site design codes

This chapter sets out the design codes for one site allocation and two more potential sites. This also includes work to test the capacity of each respective site. These Site Design Codes are a valuable tool in securing context-driven, high quality development on the sites, reflecting local aspirations for high-quality sustainable development in Tadcaster.

## 5.1 Introduction

This chapter is divided into three more parts, with one sub-section for each site under consideration.

The sites are as follows;

- Site 1: Land North of Station Road;
- Site 2: Land at Mill Lane (former Renaissance Project site); and
- Site 3: Central Area Car Park

Where possible, images of existing good character developments from Tadcaster are used to express the design principles. Where these images are not available, the following outputs are used; descriptive text, images of best practice examples, illustrations and explanatory diagrams.



F.92

Figure 92: Land off Station Road.



F.93

Figure 93: Potential Mill Lane site.



F.94

Figure 94: Town centre car park site.



## 5.2 Design principles for: Land North of Station Road

This area consists of the local plan allocated site at the western gateway to the town, adjacent to Inholmes estate. It is proposed that the design of the site can improve on somewhat sympathetic design approaches of previous private housing developments from the 1970's in the area, referring to hillside lanes, the adjacent Inholmes estate and position at the western gateway in relationship to the greenbelt. A high-quality design with outward facing, characteristic development in a green setting.



Figure 95: Site location map

### 5.1.1 Opportunities & Constraints

The following is a list of potential opportunities and constraints for the site that will require further technical consideration and prioritising by the design team:

#### Opportunities:

- To deliver attractive housing at the western gateway to Tadcaster that draws on local character precedents;
- Retaining planting on the site boundaries to benefit from a mature planted setting/screening;
- Defining the Station Road frontage with outward facing development;
- Provision of public open space on the higher (drier) areas with potential views;
- Attractively landscaped SuDs and swales draining to the lower area of the site;
- Multiple pedestrian access points will be possible to link to existing desire lines at site frontage; and

- The site has an excellent southerly aspect for solar orientation of main facades / solar energy generation (photovoltaics on roofs);

#### Constraints/ considerations:

- Vehicle access from the Station Road will need to consider the spacing to other junctions, notably Cedar Drive / Station Rd;
- Potential underground utilities and way-leaves across the site are currently unknown;
- The natural site gradients should be considered for the design of streets, paths and buildings;
- No known public access routes to the east of the site to Inholmes Lane is unfortunate; and
- Sensitive boundaries to existing properties to the north-east. Although private views are not protected, potential issues of overshadowing and/or overlooking (to and from new development) must be addressed carefully.



F.96

Figure 96: Indicative opportunities and constraints plan

## Design Principles for Land North of Station Road:

- Set development height and scale within the mature landscape setting of the hillside. Retain existing trees on the site boundaries and enhance boundary vegetation / hedgerow;
- Dwellings should align with the topography and drainage pattern of the site, allowing space for SuDS and swales (circa 10% of the gross site area) on the lower southern edge;
- Green verges should be retained along the Leeds Road and station Road, integrating new footpaths also where appropriate. Retain and reinforce hedgerow along the Station Road;
- Homes should be outward facing (locating main entrances) to Leeds Road / Station Road;
- Consider a rubble stone boundary wall along the site frontage to the Leeds Road/Station Road; No close-board fencing should be presented to external site boundaries or new streets;
- The net site density should be a minimum of 30 DpH in line with local plan policy and include a mix of housing types and be open to a number

of different tenures, including up to 40% affordable housing in line with local plan policy;

- Materials and style of home should respond to local character with stone masonry construction and red pantile roofs being the default local style;
- Access streets should be based on traditional hillside lanes, and respond to the topography;
- Development layout should be an informal pattern with a variety of spatial enclosures and dynamic views within to considered street-scenes, building arrangements and landscape;
- Standardised suburban street types with oversized junctions/ turning areas are not allowed;
- Building positions should define the streets and spaces between them, not the other way around (in other words buildings are not simply offset from edges of suburban roads);
- Buildings heights and scales should vary from 1.5 – 2 storey with a varied roofscape and storey heights that are sympathetic to the edge of town, semi-rural transition;
- A portion of the site should be retained as open space in line with local plan

policy (in addition to space for SuDS);

- Plot boundaries to the front of homes may be hedgerows, shrub planting, estate rail or low masonry walls. Rear or side boundaries facing the public realm should be masonry walls that match main building material (not closed-board fence);
- Large tarmac areas are unattractive and block paving or crushed rolled stone in bitmac should be used in key areas to improve visual appearance of public spaces, paths or junctions;
- Street design must have a 20mph maximum design speed with street segments less than 70m in length. Homezone areas, shared surface lanes and private drives for slow pace movement should be incorporated in low traffic areas, e.g. edge lanes);
- Horizontal speed restraint measures, including visual narrowing of carriageway, on street parking bays, landscaping and sets across the carriageway in bands are allowed. Raised table junctions for level pedestrian crossing are preferable to speed humps; and
- Footways can be on one-side only in where dwellings are not directly accessed from both sides of the street.



If parking is provided to the rear of properties then it must be well overlooked and include landscaping and high quality public space

Pavements can be one side only in specific circumstances

Retained existing trees / hedgerow boundaries

Potential SuDS and swales

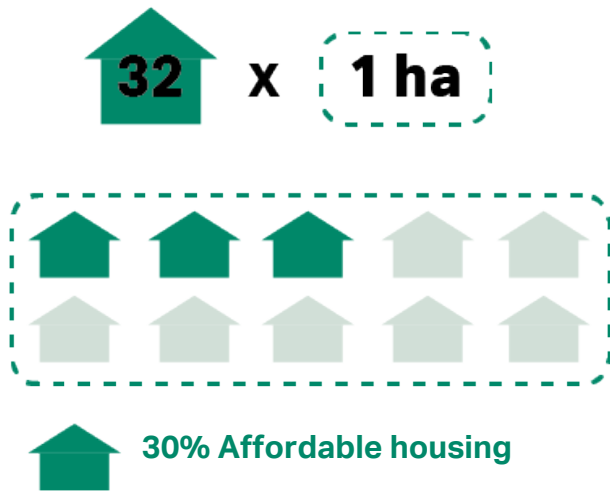
**Figure 97:** Indicative draft layout for exploration of potential design issues and opportunities. This layout does not reflect a preferred housing mix for development which should be based on local need. The Local Plan suggests the site can accommodate up to 104 dwellings which is based on a flat density of 30DpH across the whole site. This layout includes open space, green margins, SuDS so as to achieve biodiversity net-gain.

Building positions define the street scene

Rolled stone in carriageways or footways improves appearance

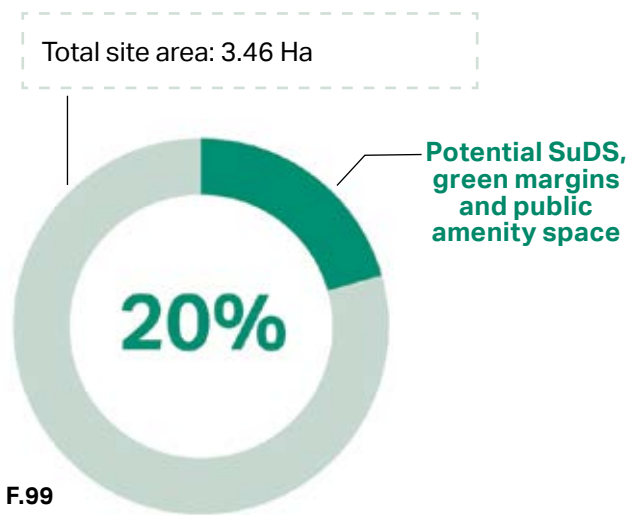
Outward facing development on Station Road and Leeds Road creates positive street scenes

Retained existing trees / hedgerows along Station Road and Leeds Road



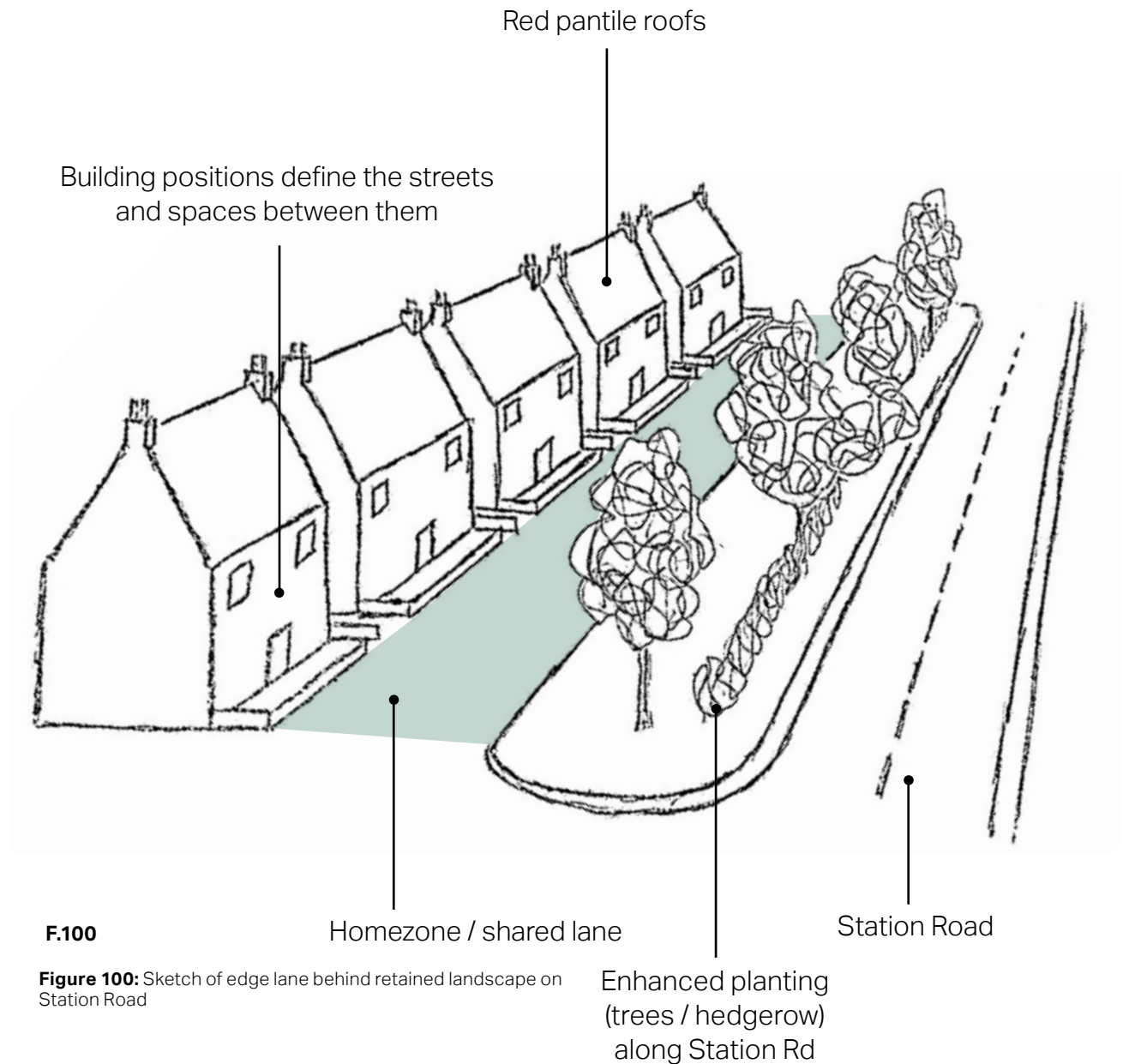
F.98

Figure 98: Indicative density and affordable housing requirement



F.99

Figure 99: Indicative green space proportion to consider for biodiversity net-gain and



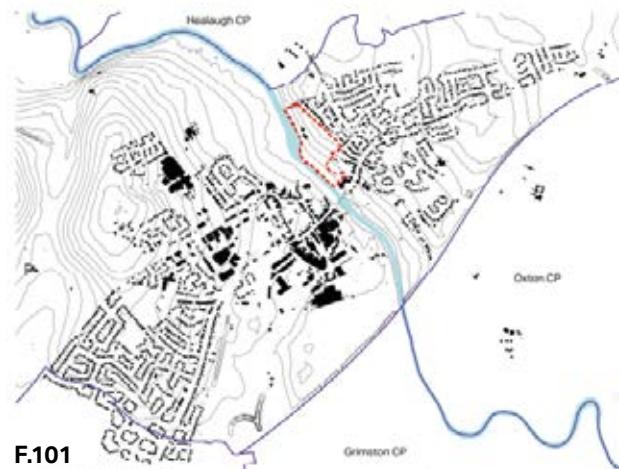
F.100

Figure 100: Sketch of edge lane behind retained landscape on Station Road

Enhanced planting (trees / hedgerow) along Station Rd

### 5.3 Design principles for: Land at Mill Lane (former Renaissance Project site):

The Mill Lane site on the eastern bank of the river is a very important potential housing site for Tadcaster. Part of the site has planning permission already. The latter is owned by the brewery and has outline planning consent for housing. This site could include the PowerPlus site on Commercial Street (not included in local plan allocation). This site is circa 3Ha and suitable for mixed-use including residential. The local plan indicates a density of up to 80 dpH which would mean a high-density scheme that is unlikely to fit the townscape character and sensitive riverside context.



**Figure 101:** Site location map



**F.103**  
**Figure 103:** Lithograph of Tadcaster Mill (published 1868)



**Figure 102:** Extract of URBED masterplan for Mill Lane housing site (Renaissance project site) from Selby District Renaissance Strategic Development Framework

#### Design Principles for Land at Mill Lane:

- The edge response requires an attractive, outward facing development to the valley as it is highly visible from the riverside, church grounds, viaduct and bridge. These key views must be considered;
- The site could incorporate additional flood defence measures. Homes must be outside flood zones 2 and 3 or have defences to provide protection. Access from the top of the site should be considered if riverside access routes have potential to flood;
- Consider tree planting and green walls on the site boundary to the river and enhance bio-diversity, also understand whether planting can integrate flood defenses more sensitively;
- The scheme must incorporate a riverside walk and consideration to keep parking, vehicle traffic off the riverside frontage should be taken;
- The housing must be high quality urban riverside / mill site housing exemplifying traditional scale and local character. Innovative designs that incorporate energy saving measures should still relate to town character, the riverside location, the former mill

and conservation area setting.

- Development should include a range of tenures and types of accommodation including family housing to affordable apartments;
- Homes should be outward facing to the riverside and complete adjacent development plots, blocks and boundaries so as to provide security, attractive boundaries and make key pedestrian links safe and secure;
- High-quality stone plot boundaries to the public realm are required in key locations. Likewise, any retaining wall designs must be attractively designed to help form an attractive public realm, where required;
- The green corridor along the riverside, should be enhanced with new planting, and integrate new footpaths as part of the riverside walking route;
- Dwellings should respect the topography and drainage pattern of the site (towards the river); Great attention should be paid to roofscape and building alignment, predominantly it should be parallel to the river to settle and integrate development along the hillside;
- Access streets should be robust and characterful urban streets, directly

addressing the topography to make the most of the site area, whilst achieving required street gradients for access;

- Aim for tighter urban street geometry with stronger sense of street enclosure. Standardised suburban streets with oversized junction turning spaces are not appropriate but slope gradient effects must be considered in vehicle tracking and junction geometry;
- Development layout should be a semi-formal pattern with an urban spatial enclosure and views back to the riverside the main focus of the development and sense of place;
- Building position should define the streets and spaces in line with topography, to which routes are to be aligned carefully;
- Buildings heights should vary from 2 – 3 storeys with a well-designed roofscape and storey heights that are sympathetic to the riverside/ hillside locations and adjacent development. Where mixed or commercial uses are incorporated into the ground floor or historic building precedents are used then 3 storeys may be appropriate;
- The density of the site should not be

less than 50 DpH and include a mix of house types and apartments with a number of different tenures, including up to 40% affordable housing (subject to local plan policy);

- A portion of the site should be retained as open space along the riverside to retain an attractive and functional (for ecology) green / blue corridor;
- high quality boundaries (e.g. masonry walls) with gates should be used to rear gardens or parking courts where used;
- Streets and paths should use areas of stone sets and rolled stone in key areas (e.g. riverside path) to improve visual appeal of public spaces and to increase friction/ safety on steeper sloping streets or pathways for pedestrians, cyclists and vehicles;
- The development should be designed to a 20mph maximum design speed with homezone areas / shared surfaces for walking pace movement and short street segments (up to 70m length).
- Horizontal speed restraint measures are allowed, including visual narrowing of carriageway, on street parking bays, landscaping and sets within the carriageway in line with the urban - riverside character.

### Indicative minimum site density

**50** x **1 ha**



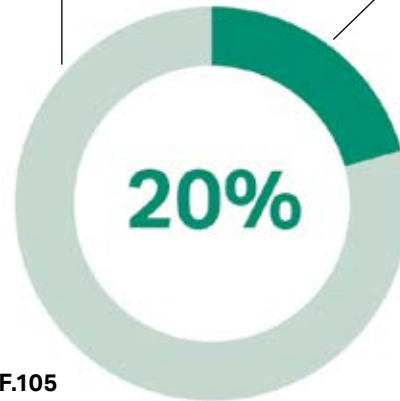
**30% of affordable housing**

**F.104**

**Figure 104:** Indicative density and affordable housing proportion

### Open space and SuDS

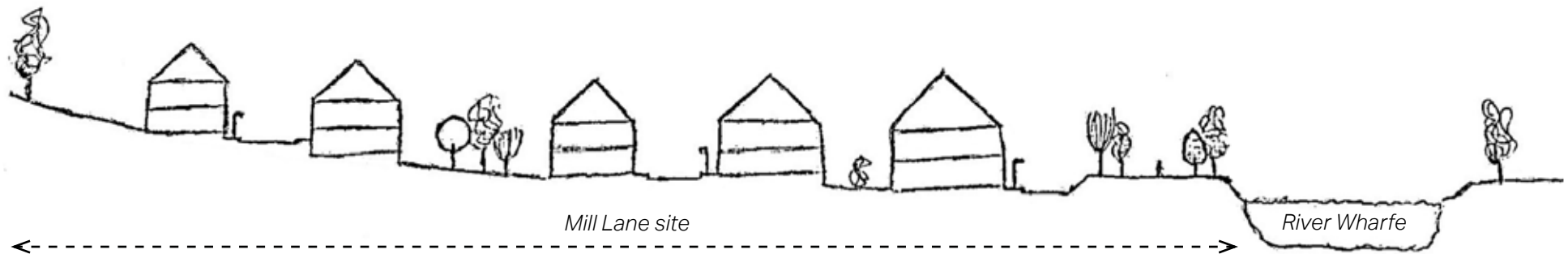
Total site area: 3.03Ha



Potential riverside green / blue corridor, amenity space and SuDS

**F.105**

**Figure 105:** Indicative green space proportion to consider



**F.106**

**Figure 106:** Indicative site section through Mill Lane site (detailed site sections will be key to designing a successful scheme that fits into its sensitive riverside context)



## 5.4 Design principles for: Central Area Car Park

This infill opportunity on this main town centre site is a major development opportunity (circa 40 units) in a sensitive urban context within the conservation area. This is dependent on finding a new site to replace the car parking which is important to the commercial success of the town centre and supporting the range of independent businesses. Repurposing this previously developed land (a brownfield site) is preferable to speculative development within the green belt. The following design principles apply to major urban infill development that may come forward within the plan period, via allocation or appeal, on the Central Area Car Park.



F.107

Figure 107: Site location map

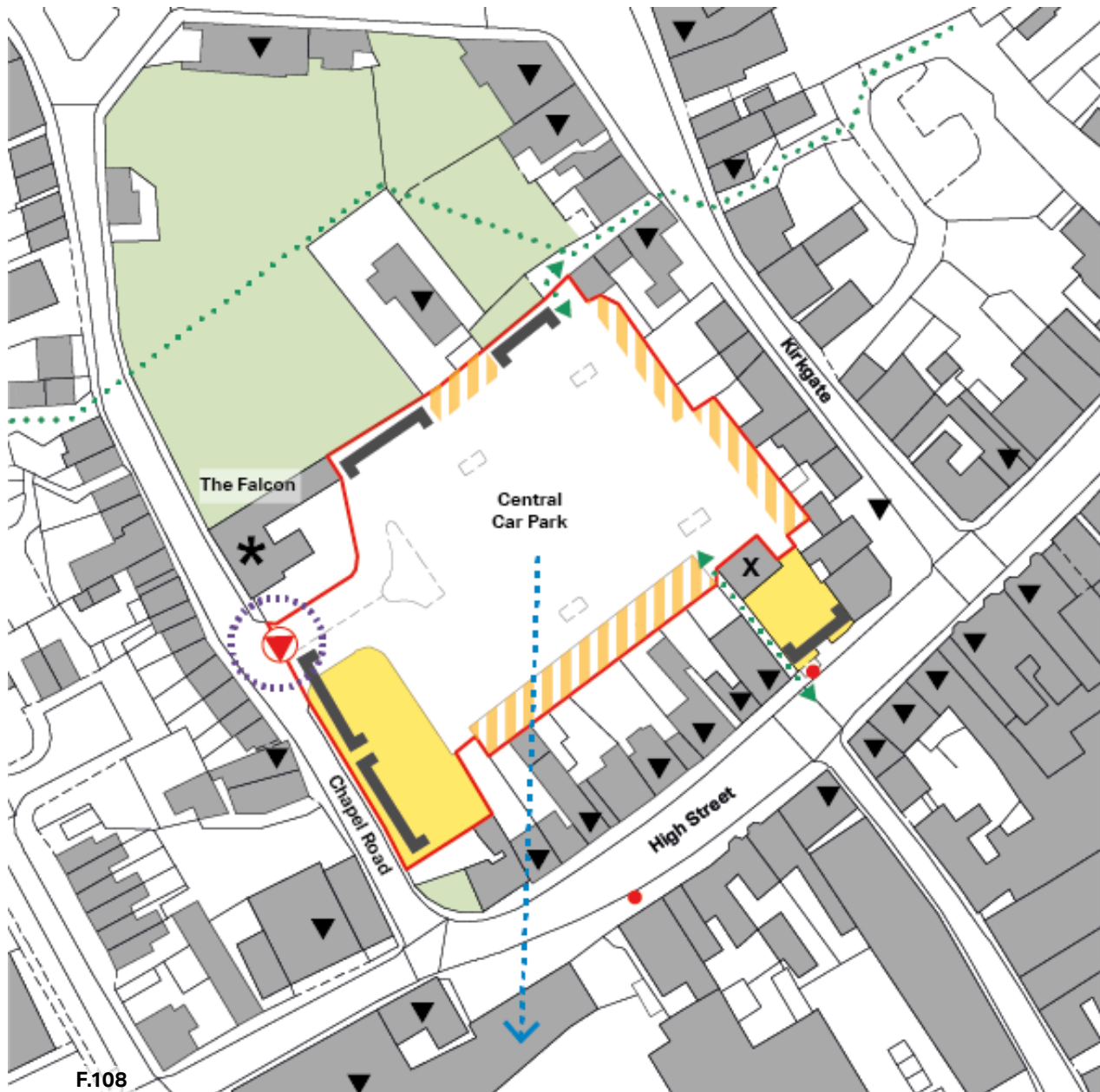
### 5.4.1 Opportunities and Constraints

#### Opportunities:

- To deliver characterful housing in Tadcaster's town centre that draws on local character precedents and contributes positively to the historic character and appearance of Tadcaster Conservation Area;
- Planting new trees and vegetation in the town centre of Tadcaster;
- Defining the Chapel Road frontage with outward facing development;
- Multiple pedestrian access points will be possible to link to Chapel Road, High Street and Kirkgate;
- Potential inclusion of new public open space on the site;
- Potential green and blue infrastructure including: rain gardens, green roofs, permeable paving and soakaways;
- Infill sites (Chapel Road and High Street) south-east and south-west of the site integrated with the allocation;

#### Constraints/ considerations:

- Existing Public Right of Way through Former Vicarge Gardens, is currently prevented;
- Potential underground utilities and way-leaves across the site are currently unknown;
- The natural desire lines through the site should be considered for the design of streets, paths and buildings;
- Consider potential view to brewery chimney;
- Sensitive existing property boundaries to southern, eastern and northern boundaries. Although private views are not protected, potential issues of overshadowing and/or overlooking (to and from new development) must be considered and addressed; and
- Property boundaries to the south and east have existing access (gates in boundary walls) that will need to be retained.



- Site boundary
- Pedestrian access
- ▲ Potential vehicle access
- Key site frontage
- View to brewery chimney
- Buildings
- Important boundary
- \* Public house
- Sensitive edge
- Private back gardens
- ▼ Listed buildings
- Amenity space
- Infill site
- Gateway to site
- Public Right of Way
- X Vacant building
- Bus stop

F.108

Figure 108: Indicative opportunities and constraints plan



Figure 109: Historic map extract showing previous building pattern on site

## Design Principles for Chapel Street / Central Area Car Park:

- Access to development for vehicles will be via Chapel Street. The scale of access must be in keeping with the narrower, more enclosed scale of historic streets within the town centre;
- Access and space for servicing must be provided for existing businesses on Kirkgate and the High Street. New development must not compromise existing amenity of any dwellings;
- New development should contribute positively to the historic character and appearance of Tadcaster Conservation Area and its setting, taking into account the significance of both listed buildings and local heritage assets;
- The architecture of the new development should complement the mix of styles (mainly Georgian) of architecture that give the town centre its positive historic characteristics;
- Connectivity through the site for pedestrians and cyclists (including wheelchairs and mobility vehicles) must be retained via existing links to; Chapel Street, the High Street and Kirkgate;
- Any public amenity space within the

site should be linked to the former Vicarage Gardens to the north-west of the site. Public access to and enhancement of existing green space at Vicarage Gardens for new development may be a substitute for providing open space on site;

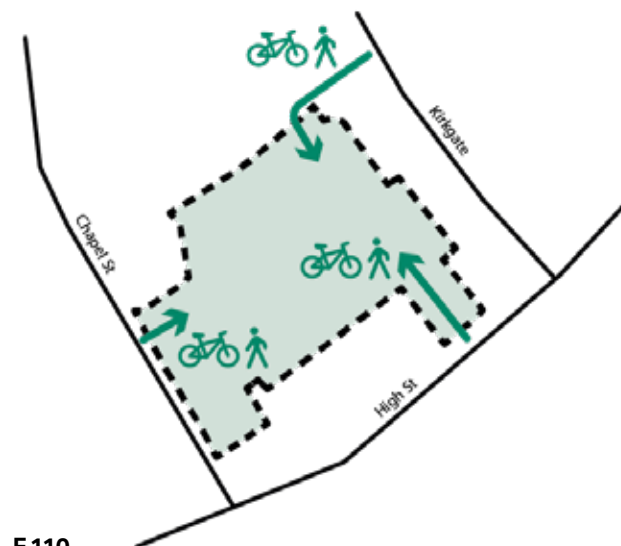
- The identity for new development must link to the town, including connections that can be made to; its rich history, the current townscape and sensitive response to the conservation area;
- Townscape design and consideration of micro-siting buildings and design elements to create gateways to development, street enclosure with varied spatial qualities (for example pinch-points or capping views with focal buildings) will aid a safe and legible built-environment;
- Street-scene design within the site should be based on the traditional lower-order streets, lanes and spaces of the town, providing an intimate scale, strong sense of enclosure and robust, attractive materials/ details;
- Serial vision studies should help determine the built-form response so that spaces unfold as a sequence of different scenes that inform and delight the user. This will create memorable spaces and places that

add to the distinctiveness of the towns organic pattern;

- Landscaping and boundaries must clearly define public and private space. Masonry walls should define side and rear plot boundaries where these are exposed to the public realm. On narrower streets then buildings may be set close to the back of pavement and have a small privacy strip of a different material that allows opportunity for personalisation without a full front garden;
- Homezones consisting of shared surface lanes or courtyards for walking pace movement of people and cars will be suitable within this tight urban context where the maximum design speed is likely to be significantly under 20mph;
- Parking should be set to a maximum standard of 1 space per dwelling based on the town centre location and walkable distance to the nearby bus station. Cycle parking / bicycle storage should be provided on plot in a convenient location for all dwellings;
- Public realm design must create a high-quality environment which transforms the quality of the current environment and adds to the quality of the town centre and conservation area;

- A mix of housing to create a balanced community, including affordable properties for local people;
- New housing must create an attractive, safe and distinctive urban neighbourhood that adds vitality to the town centre through additional footfall and the presence of doors on streets;
- Generic, house-builder product is not appropriate in Tadcaster and the design approach must utilise one of the following responses; (1) Authentic reproduction of historic styles - most expensive and difficult to achieve (cannot be bolt-on pastiche) ; (2) Complementary and contextual design that acknowledges traditional materials, references local character and scale in a contemporary way, without slavishly reproducing the past; or; (3) Innovative design that is progressive and introduces new building technology and materials in a sensitive way that may contrast positively with historic townscape whilst adding a valuable precedent for the future in terms of combatting climate change and future-proofing the town centre;

- Higher density development so that people are within walking distance of town centre services and facilities and bus station is appropriate but must be balanced with a sensitivity to the existing grain of the town which is mixed but has local patterns to reference;
- Plot width and building height are critical to defining appropriate scale building frontages;
- Main building heights must be between 2-3 storeys. Outbuildings and extensions may be single storey where required;
- Building depth/ width is critical to maintaining the built-form pattern of Tadcaster and the impact of new development should be compared with the existing townscape via figure ground studies; and
- Private outdoor space is essential for all new homes and either requires space on plot or innovative provision within courtyards or roof gardens that sensitively integrate with the roofscape of the town centre.



F.110

Figure 110: Pedestrian and cycle accesses to the site.



F.111

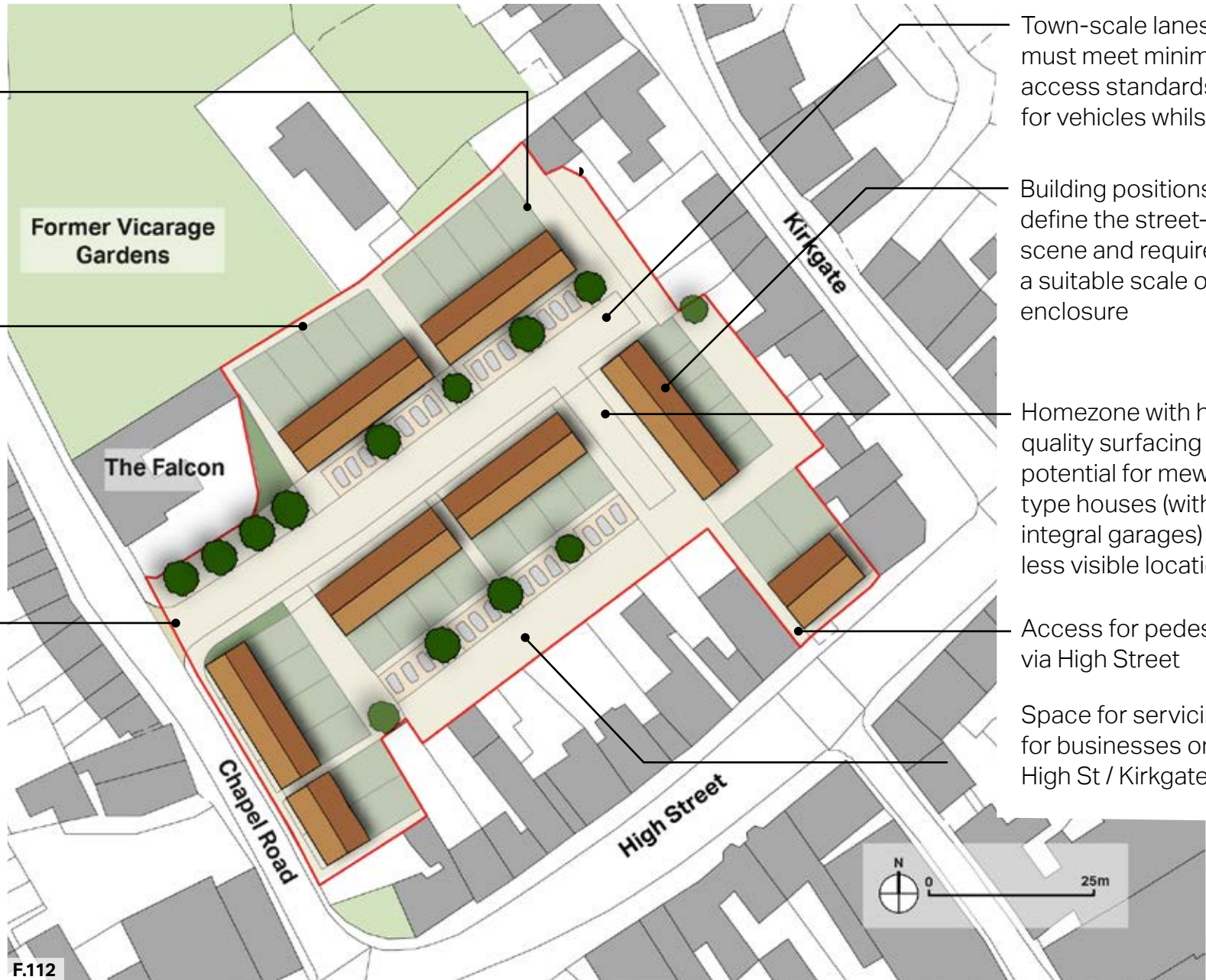
Figure 111: Link to high Street via vacant plot (note bus stop on site frontage to consider)

Planting, buildings and boundaries define public and private spaces

Existing properties tend to back on to the former Vicarage Gardens but attractive rear facades would be needed

Access for vehicles via Chapel Street

**Figure 112:** Indicative draft masterplan for exploration of design issues only. This layout does not reflect a preferred housing mix for development. The number of units shown may vary dependent on mix and size of units required. The local plan suggests an indicative capacity of up to 43 homes which would require a greater proportion of smaller homes/ apartments than depicted here, at an average of 65 DpH. The scheme illustrated is circa 50 DpH.



Town-scale lanes must meet minimal access standards for vehicles whilst

Building positions define the street-scene and require a suitable scale of enclosure

Homezone with high quality surfacing and potential for mews type houses (with integral garages) in less visible locations

Access for pedestrians via High Street

Space for servicing for businesses on High St / Kirkgate

F.112

Indicative minimum site density

50 x 1 ha



30% of affordable housing

F.113

Figure 113: Potential density and affordable housing provision to consider



F.114

Figure 114: Contemporary house design with roof terrace



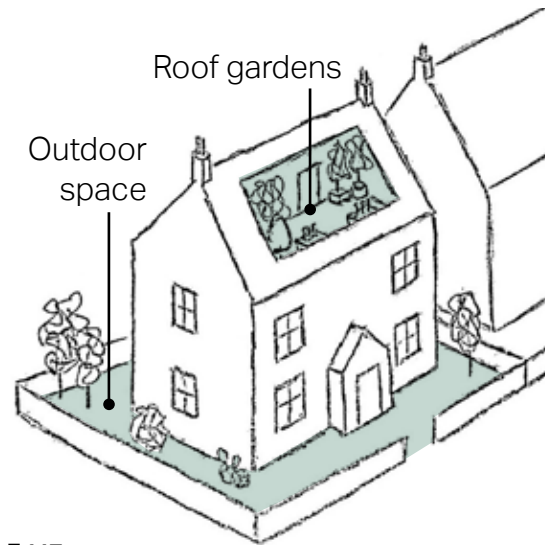
F.115

Figure 115: Modern mews house



F.116

Figure 116: View south along Chapel Street including existing site access on the left



F.117

Figure 117: Precedent images for alternative amenity space provision with private outdoor space in the form of roof gardens



**Appendix**

**06**

# 6. Appendix

This appendix sets out a general list of design considerations by topic as a quick reference guide to use in workshops and discussions.

## 1 General design guidelines for new development:

- Integrate with existing paths, streets, circulation networks and patterns of activity;
- Reinforce or enhance the established settlement character of streets, greens, and other spaces;
- Harmonise and enhance existing settlement in terms of physical form, architecture and land use;
- Relate well to local topography and landscape features, including prominent ridge lines and long-distance views;
- Reflect, respect, and reinforce local architecture and historic distinctiveness;
- Retain and incorporate important existing features into the development;
- Respect surrounding buildings in terms of scale, height, form and massing;
- Adopt contextually appropriate materials and details;
- Provide adequate open space for the development in terms of both quantity and quality;
- Incorporate necessary services and drainage infrastructure without causing unacceptable harm to retained features;
- Ensure all components e.g. buildings, landscapes, access routes, parking and open space are well related to each other;
- Positively integrate energy efficient technologies;
- Make sufficient provision for sustainable waste management (including facilities for kerbside collection, waste separation, and minimisation where appropriate) without adverse impact on the street scene, the local landscape or the amenities of neighbours;
- Ensure that places are designed with management, maintenance and the upkeep of utilities in mind; and
- Seek to implement passive environmental design principles by, firstly, considering how the site layout can optimise beneficial solar gain and reduce energy demands (e.g. insulation), before specification of energy efficient building services and finally incorporate renewable energy sources.



## 2

### Street grid and layout:

- Does it favour accessibility and connectivity? If not, why?
- Do the new points of access and street layout have regard for all users of the development; in particular pedestrians, cyclists and those with disabilities?
- What are the essential characteristics of the existing street pattern; are these reflected in the proposal?
- How will the new design or extension integrate with the existing street arrangement?
- Are the new points of access appropriate in terms of patterns of movement?
- Do the points of access conform to the statutory technical requirements?

## 3 (continues on next page)

### Local green spaces, views & character:

- What are the particular characteristics of this area which have been taken into account in the design; i.e. what are the landscape qualities of the area?
- Does the proposal maintain or enhance any identified views or views in general?
- How does the proposal affect the trees on or adjacent to the site?
- Can trees be used to provide natural shading from unwanted solar gain? I.e. deciduous trees can limit solar gains in summer, while maximising them in winter.
- Has the proposal been considered within its wider physical context?
- Has the impact on the landscape quality of the area been taken into account?
- In rural locations, has the impact of the development on the tranquillity of the area been fully considered?
- How does the proposal impact on existing views which are important to the area and how are these views incorporated in the design?
- How does the proposal impact on existing views which are important to the area and how are these views incorporated in the design?
- Can any new views be created?
- Is there adequate amenity space for the development?
- Does the new development respect and enhance existing amenity space?

# 3

## Local green spaces, views & character:

- Have opportunities for enhancing existing amenity spaces been explored?
- Will any communal amenity space be created? If so, how this will be used by the new owners and how will it be managed?
- Is there opportunity to increase the local area biodiversity?
- Can green space be used for natural flood prevention e.g. permeable landscaping, swales etc.?
- Can water bodies be used to provide evaporative cooling?
- Is there space to consider a ground source heat pump array, either horizontal ground loop or borehole (if excavation is required)?

# 4

## Gateway and access features:

- What is the arrival point, how is it designed?
- Does the proposal maintain or enhance the existing gaps between settlements?
- Does the proposal affect or change the setting of a listed building or listed landscape?
- Is the landscaping to be hard or soft?

# 5 (continues on next page)

## Buildings layout and grouping:

- What are the typical groupings of buildings?
- How have the existing groupings been reflected in the proposal?
- Are proposed groups of buildings offering variety and texture to the townscape?
- What effect would the proposal have on the streetscape?
- Does the proposal maintain the character of dwelling clusters stemming from the main road?
- Does the proposal overlook any adjacent properties or gardens? How is this mitigated?

# 5

## Buildings layout and grouping:

- Subject to topography and the clustering of existing buildings, are new buildings oriented to incorporate passive solar design principles, with, for example, one of the main glazed elevations within 30° due south, whilst also minimising overheating risk?
- Can buildings with complementary energy profiles be clustered together such that a communal low carbon energy source could be used to supply multiple buildings that might require energy at different times of day or night? This is to reduce peak loads. And/or can waste heat from one building be extracted to provide cooling to that building as well as heat to another building?

# 6

## Building line and boundary treatment:

- What are the characteristics of the building line?
- How has the building line been respected in the proposals?
- Has the appropriateness of the boundary treatments been considered in the context of the site?

# 7

## Building heights and roofline:

- What are the characteristics of the roofline?
- Have the proposals paid careful attention to height, form, massing and scale?
- If a higher than average building(s) is proposed, what would be the reason for making the development higher?
- Will the roof structure be capable of supporting a photovoltaic or solar thermal array either now, or in the future?
- Will the inclusion of roof mounted renewable technologies be an issue from a visual or planning perspective? If so, can they be screened from view, being careful not to cause over shading?

# 8

## Household extensions:

- Does the proposed design respect the character of the area and the immediate neighbourhood, and does it have an adverse impact on neighbouring properties in relation to privacy, overbearing or overshadowing impact?
- Is the roof form of the extension appropriate to the original dwelling (considering angle of pitch)?
- Do the proposed materials match those of the existing dwelling?
- In case of side extensions, does it retain important gaps within the street scene and avoid a 'terracing effect'?
- Are there any proposed dormer roof extensions set within the roof slope?
- Does the proposed extension respond to the existing pattern of window and door openings?
- Is the side extension set back from the front of the house?
- Does the extension offer the opportunity to retrofit energy efficiency measures to the existing building?
- Can any materials be re-used in-situ to reduce waste and embodied carbon?

# 9 (continues on next page)

## Building materials & surface treatment:

- What is the distinctive material in the area?
- Does the proposed material harmonise with the local materials?
- Does the proposal use high-quality materials?
- Have the details of the windows, doors, eaves and roof details been addressed in the context of the overall design?
- Does the new proposed materials respect or enhance the existing area or adversely change its character?
- Are recycled materials, or those with high recycled content proposed?

# 9

## Building materials & surface treatment:

- Has the embodied carbon of the materials been considered and are there options which can reduce the embodied carbon of the design? For example, wood structures and concrete alternatives.
- Can the proposed materials be locally and/or responsibly sourced? E.g. FSC timber, or certified under BES 6001, ISO 14001 Environmental Management Systems?

# 10

## Car parking:

- What parking solutions have been considered?
- Are the car spaces located and arranged in a way that is not dominant or detrimental to the sense of place?
- Has planting been considered to soften the presence of cars?
- Does the proposed car parking compromise the amenity of adjoining properties?
- Have the needs of wheelchair users been considered?
- Can electric vehicle charging points be provided?
- Can secure cycle storage be provided at an individual building level or through a central/ communal facility where appropriate?
- If covered car ports or cycle storage is included, can it incorporate roof mounted photovoltaic panels or a bio-diverse roof in its design?

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